

# CITY OF MARLBOROUGH MEETING POSTING

Meeting Name: City Council Urban Affairs Committee

Date: May 15, 2018

Time: 5:45 PM

Location: City Council Chamber, 2<sup>nd</sup> Floor, City Hall, 140 Main Street

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2018 MAY -9 P 1:26

Continued Review of the Metropolitan Area Planning Council Housing Guidelines associated with **Order No. 17/18-1006979B**: Communication from Metropolitan Area Planning Council regarding 1.) Multifamily Design Review Guidelines; 2.) Multifamily Development Review Criteria; 3.) Background information on TDR; 4.) MA Subsidized Housing Inventory for Marlborough; and 5.) Housing Needs Assessment related to the Housing Study Report.

-REFER TO URBAN AFFAIRS

New Business:

01-08-2018 – **Order No. 18-1007137 (X17-1006983A)**: Communication from Attorney Falk on behalf of David Skarin, regarding Proposed Zoning Amendment, Rail Trail Overlay District, 19 Ash Street.

-REFER TO URBAN AFFAIRS & PLANNING BOARD

-PUBLIC HEARING: MARCH 5, 2018

THE LISTING OF TOPICS THAT THE CHAIR REASONABLY ANTICIPATES WILL BE DISCUSSED AT THE MEETING IS NOT INTENDED AS A GUARANTEE OF THE TOPICS THAT WILL HAVE BEEN DISCUSSED. NOT ALL TOPICS LISTED MAY IN FACT BE DISCUSSED, AND OTHER TOPICS NOT LISTED MAY ALSO BE BROUGHT UP FOR DISCUSSION TO THE EXTENT PERMITTED BY LAW.

**The public should take due notice that the Marlborough City Council may have a quorum in attendance due to Standing Committees of the City Council consisting of both voting and non-voting members. However, members attending this duly posted meeting are participating and deliberating only in conjunction with the business of the Standing Committee.**

**Electronic devices, including laptops, cell phones, pagers, and PDAs must be turned off or put in silent mode upon entering the City Council Chamber, and any person violating this rule shall be asked to leave the chamber. Express authorization to utilize such devices may be granted by the President for recordkeeping purposes.**

City of Marlborough  
**Multifamily Development Review  
Criteria and Design Guidelines**

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*DRAFT FOR DISCUSSION*  
APRIL 26, 2018



## **Marlborough Multifamily Development Review Criteria and Design Guidelines**

The City of Marlborough Multifamily Design Guidelines (MDG) are intended to assist the City Council and Urban Affairs Committee with the review of multifamily development that may be proposed throughout the City.

The document is also intended to provide multifamily development teams with an indication of the types of projects that may be deemed suitable for specific areas of the City and to communicate the types of features that are desired by the City for investments to successfully contribute to the community.

Questions or comments relating to the multifamily development review criteria and design guidelines should be directed to the Marlborough Economic Development Corporation (MEDC).



# Marlborough Multifamily Development Review Criteria

## Purpose

The purpose of these Multifamily Development Criteria is to assist developers and the City of Marlborough in the appropriate design and municipal review of multifamily developments proposed in the City. The intent is to provide information to prospective developers regarding the scale, type, design, tenure, and municipal benefits related to multifamily development the City prefers within the various areas and neighborhoods of the City, so that the developments may be designed in a manner that meets municipal goals and needs. Developments that do not meet these standards may not receive the necessary approvals for zoning changes and/or special permits for development from the City Council.

The City of Marlborough recognizes that residential development is beneficial to the City:

- To meet the housing needs of the current and projected population growth of the City and the region,
- To provide housing for the future residents/employees needed to continue strong regional and local economic growth, and
- To provide residents whose purchasing power will support the economic vitality of the City's retail and commercial establishments and districts.

Based upon the finding of the 2013 *Marlborough Housing Supply/Demand Needs Analysis* (prepared by Metropolitan Area Planning Council - MAPC) and the 2017 *Multifamily Market and Fiscal Impact Analysis* (prepared by RKG Associates), the City of Marlborough supports proposals for well-constructed and designed residential development that are in keeping with the high standards of the municipality and which meet the contextual design of, and have a positive impact on, the neighborhoods in which the development is proposed.

## Development Review Criteria

When evaluating Multifamily Residential Developments in the City, the following questions, without limitation, will be used to evaluate the project under the Special Permit provisions of the zoning ordinance. Project proponents should provide answers to these questions prior to meeting with the City to discuss the proposal. The city may also use this form in evaluating the proposals:

**1. Design Guidelines:** Does the proposed development meet the Multifamily Development Design Guidelines (beginning on Page 10 of this document) for multifamily development, including both the type of structure and the design details, for the neighborhood or neighborhood context type? For larger developments, does the proposed development provide a diversity of housing types/unit mixes<sup>1</sup>, etc. to ensure that it provides for a diversity of residential types as recommended by past planning studies? Copies of plans should be provided for review.

Points

<i>Proposal corresponds to Design Review Guidelines (DRG)</i>	<b>30 points</b>
<i>Plan somewhat/partially consistent with DRG</i>	<b>10 points</b>
<i>Plan not consistent with DRG</i>	<b>0 points</b>

<i>Plan has a diversity of types/units</i>	<b>5 points</b>
<i>Plan does not contain diversity of types/units</i>	<b>0 points</b>



**2. Context Sensitive:** Is the overall site design of the development respectful of the neighborhood, inclusive of appropriate landscaping and park space<sup>2</sup> for residents and guests, and one that integrates parking within an attractive layout that supports walkability? The proposal should detail how the development meets the standards set forth in these Criteria, the Design Guidelines, and the other provisions of the City Code<sup>3</sup>. Does the development provide adequate buffer to adjacent residential uses, or does it incorporate lower density/scale elements (e.g., townhomes) to provide a buffer for adjacent uses? Multifamily developments that are proposed at the edges of different types of land uses (e.g., between a commercial/office area and a single family neighborhood) should be designed so that the multifamily development type proposed (see Design Guidelines) is appropriate for the lower-intensity land use (e.g., in the commercial/single family edge example above, the development should be designed to fit with the single family development). In cases where the site to be developed is larger, then a gradation of building types may be appropriate, with lower scale development near the abutting lower density adjacent uses, to provide a buffer.

Points

Is the development proposal appropriate in scale or provide a buffer to adjacent residential uses?

Yes 10 points  
 No 0 points

If this is a case of a re-zoning or variance, does the proposed development have more, or less, adverse impact on the existing surrounding land uses than development that would otherwise be allowed by right on the same property (e.g., does the multifamily development fit better than an office or industrial project that may be allowed under existing zoning).

Less impact 10 points  
 About the same 0 points  
 More impact -10 points

Is the development's parking appropriately located to ensure easy walkability to residences, does not form a barrier between sidewalk and any first-floor commercial space in mixed use developments, and is screened from abutting uses?

Yes 10 points  
 Partially 5 points  
 No 0 points

<sup>1</sup> Housing type/unit mixes may include such items as live-work units, universal-design units, studios, units with varying numbers of bedrooms, etc.

<sup>2</sup> For instance, Zoning Code Section 650-40 F (8) states "In all districts in which multifamily dwellings are allowed, there shall be provided with each apartment building a landscaped area equal to the greatest single floor area of the building." Landscaping requirements are also included in Zoning Code Section 650-47.

<sup>3</sup> See Marlborough Code 270-2 Site Plan Review and Approval, sub-section D Site Plan Review Criteria, for a list of design elements and standards that should be incorporated into any multifamily proposal review.

**3. Land Use Compatibility:** If proposing a re-zoning to enable residential development – does the proposed project fit with, or conflict with, adjacent land uses? Residential development may be compatible with adjacent retail or office or mixed uses, but may conflict with nearby heavy industrial uses (with potential negative impacts for both uses).

Points

Is there inherent conflict with abutting uses (e.g., placing residential adjacent to heavy industrial or heavy trucking)

Yes

-30 points

No

0 points

**4. Beneficial Impacts:** Will the proposed development provide beneficial impacts on abutting or nearby uses, such as providing residents/customers for nearby walkable retail districts?

Are there defined beneficial impacts on nearby uses?

Yes

10 points

No (or limited)

0 points

**5. Consistent with Vision:** Is the development proposal consistent with a vision for the area as determined by the City through a public planning process (e.g., the visioning and re-zoning process for the Marlborough Village District)?

Is the proposed development consistent, inconsistent or in conflict with a recent local planning effort?

Consistent

10 points

Inconsistent

-10 points

In conflict with

-30 points

No recent planning in area

0 points

An example being a City plan in which the total height for an area is set at 3 stories; a development proposal for 4 stories is inconsistent with the plan, but may be acceptable if there are specific circumstances related to the site and there are other benefits to the project, whereas a 7-story structure is in definite conflict with the plan.

**6. Impact Mitigation:** What are the anticipated impacts of the development (e.g., traffic, water use, sewage generation, school costs<sup>4</sup>, emergency services calls, etc.), and does the City have adequate public infrastructure for such development, or does the developer propose adequate mitigation to offset these impacts (e.g., installation of sidewalk to connect the development to existing sidewalk network to promote walkability and thereby reduce vehicular trips)?

Development Impacts				
Category of Impact	City has adequate capacity	Impact completely mitigated by developer	Impact partially mitigated by developer	Impact not mitigated by developer
Traffic/ transportation				
Municipal water capacity				
Municipal sewer capacity				
Estimated school impacts				
Emergency service calls				

Points

Summary evaluation of impact mitigation based upon above chart:

<i>City has adequate public infrastructure capacity</i>	<i>0 points</i>
<i>Developer has proposed to undertake mitigation of inadequate infrastructure to enable development</i>	<i>20 points</i>
<i>Some development mitigation provided</i>	<i>5 points</i>
<i>Developer has not proposed sufficient mitigation for inadequate infrastructure issues</i>	<i>-30 points</i>

**7. Housing Tenure:** Does the proposed housing tenure (i.e., rental versus ownership of units) meet the needs of the City as outlined in the above-referenced reports (e.g., mix of tenure within larger proposed developments), to maintain a diversity of not only housing types but also a mix of housing tenure.

Is there a mix of housing tenure within the development?

OR

Does the proposed housing tenure within the development meet the goals of the City (based upon the proposed plans and recently approved and built projects elsewhere in the City)?

Yes	<i>10 points</i>
No	<i>0 points</i>

**8. Development Benefits:** What specific benefits to the municipality is the developer proposing in exchange for the special permit for increased density (e.g., retail on ground floor on a main street/commercial area providing for tax revenue from mixed uses, improvements to nearby sidewalk network as part of development construction, establishment of public pocket-park on site, sponsorship of annual maintenance of adjacent public park, etc.)?

Points

- Are there significant benefits to the City proposed as part of the development?
- Significant* 10 points
  - More limited* 5 points
  - None or very limited* 0 points

**Existing/historic structure or iconic neighborhood feature:**

Is the development incorporating, or removing/destroying an existing structure, and what are the beneficial design elements of the new construction?

- The development is incorporating and restoring a historic structure in the development, or is moving the structure to be restored elsewhere in the City* 30 points
- The development is removing an existing non-historic structure that is abandoned or an existing detriment to the neighborhood* 15 points
- The development is preserving an iconic neighborhood feature (e.g., large specimen street tree) as an element of the proposed development* 15 points
- There is not an existing structure on the lot* 0 points
- The development is destroying a historic structure* -30 points

**Energy efficiency/sustainability:**

- The development is incorporating significant renewable energy elements and/or is incorporating design elements that would qualify the structure for LEED certification (note: developer does not need to go through the certification process)* 10 points
- The developer would not qualify for LEED certification, but is utilizing the “most efficient available” options for heating and cooling based upon infrastructure in that area of the City* 5 points
- No significant energy elements in proposed development* 0 points

<sup>4</sup> See *The Waning Influence of Housing Production on Public School Enrollment*, by MAPC, at [www.mapc.org/enrollment/](http://www.mapc.org/enrollment/) which indicates that Marlborough, from 2010 to 2016, experienced a reduction in school enrollment of 48 students (-1.05%) during the same time that 173 units were constructed (increase in 1.05%).



**9. Affordable Housing:** The City has a goal of providing adequate supply of affordable housing for its residents, and also a goal for remaining above 10% on the State's Subsidized Housing Inventory (SHI). The developer should detail how the proposed development will meet the affordable housing requirements of the City Zoning Code, using one of the three methods listed below:

A) All multifamily residential development proposals are expected to provide the minimum number of affordable units as specified by Zoning Ordinance Section 650-26 A (1) (a), equal to 15% of the total number of units in developments over 20 units; note however, that subsection 650-26 A (2) also states that the City Council may apply these same standards to developments of fewer than 20 units. Does the proposed development include the appropriate number of affordable units (as counted on the state's Subsidized Housing Inventory for the City)?

B) Zoning Code section 650-26 A (1) (i) allows the project proponent to seek a permit to construct some or all of the affordable housing units off-site. In order to maintain diversity of affordability in all neighborhoods of the City, the off-site affordable units should be constructed within the same neighborhood/area as the market rate units. Does the proposal comply with this requirement?

C) The Affordable Housing Bylaw Section 650-26 A (1) (a), does allow for a payment-in-lieu-of-units (PILU) payment, but the City's strong preference is for the production of actual affordable units to ensure that the housing needs of the community are being met, and the City's SHI total does not fall below 10%. Note that the City Code provision for PILU sets a minimum payment of \$50,000 per unit. The City recognizes that this minimum payment is far lower than the cost of providing actual units (either on-site or off-site). Therefore, if a developer proposes a PILU instead of on-site units, the City will look more favorably on proposals for special permits where the PILU offered is equal to the cost of producing units within the development (as determined by the total cost of the development – including but not limited to land, permits and design, and all construction costs) divided by the total number of units within the development.<sup>5</sup>

Points

Are the required affordable units	
<i>Within the proposed development</i>	30 points
<i>Off site</i>	20 points
<i>In cash payments at/near \$50,000/unit</i>	5 points
<i>In cash payment per calculation</i>	10 points

OR (see item #10 below)

**10. Affordable Housing (40B):** As an alternative to #9 above, is the proposed multifamily residential development a “friendly 40B” comprehensive permit proposal that includes the 20% or 25% affordability requirements of Comprehensive Permit developments? Where re-zoning to allow for residential development is proposed, which will provide significant benefits for the developers by enabling residential uses where they are not currently allowed, preference will be given to projects that propose “friendly 40B” developments which include the appropriate 20 – 25% affordable units (with the percentage based upon the affordability levels within the development).

As an alternative to #9 above:

If the developer is proposing a zoning change, is the proposed development one that is a “friendly 40B” such that units will be countable on the City’s Subsidized Housing Inventory (SHI)?

Points

Yes	30 points
No	0 points

Total Points

**SUM OF POINTS**

Maximum Possible Points	195
Minimum Possible Points	-130

<sup>5</sup> See as alternative to the above calculation, the following text from the Maynard Zoning Bylaw that uses comparable sales to set the PILU value:

Payment in lieu of units. As an alternative to construction of affordable units within the locus of the proposed development or at another locus, an equivalent payment in lieu of units (PILU) may be made to the Maynard Affordable Housing Trust Fund.

The payment shall be an amount equal to the required number of affordable housing units multiplied by the median price of a Maynard market-rate home comparable in type, size, and number of bedrooms reported for a minimum of three (3) home sales over a period of twelve (12) months prior to the date of application submission, if available. Median home cost utilized in the formula must be approved by the Maynard Affordable Housing Trust, or designee, or the Town Administrator, or designee. The applicant shall calculate the proposed sum based on an appraisal of the comparable home sales and submit documentation of the relevant data source(s) as part of the application.

If there is not a comparable housing unit, the payment shall be equal to the most current Total Development Cost as articulated in DHCD’s Qualified Allocation Plan for Low Income Housing Tax Credit, for the areas described as Within Metro Boston/Suburban Area, as adjusted for the type of project and number of units.

PILU shall not be accepted as part of rental development, either multifamily or mixed-use.



# Marlborough Multifamily Design Guidelines

## Introduction

The Multifamily Design Guidelines (MDG) intend to align residential investments with City goals, elevate the design quality of those investments, and assist in the review and approval process. Design Guidance is not provided for single family homes. The MDG build on the recently completed *Multifamily Market and Fiscal Impact Analysis* by RKG Associates in July 2017.

The design guidance is to provided for three distinct contexts within the City that have been identified and characterized. This division by context, allows specific guidelines for each district to focus on the most relevant design elements to strengthen investments in that context.

The design guidance for each type of residential area in the City is focused on sensitively responding to the context and elevating the quality of future housing investment. The design guidelines are not mandatory, but attempt to clearly articulate the components of a high quality design. The design guidelines do not modify zoning requirements.

***Requirements of the Marlborough Zoning Code and other City regulations remain in effect. All projects must comply with these requirements. The Design Guidelines are intended to offer additional information to guide the design and development of high quality projects within the regulatory constraints that have been established.***

The guidelines do not intend to encourage multifamily development across the entire City, or in neighborhoods that are predominantly comprised of single family homes. The guidelines anticipate that multifamily proposals may occur generally in many locations across the City in the future. As the specifics of future multifamily housing developments are proposed, this guidance intends to help strengthen each to be as context sensitive, high quality, and positive for the community as possible.

## Context

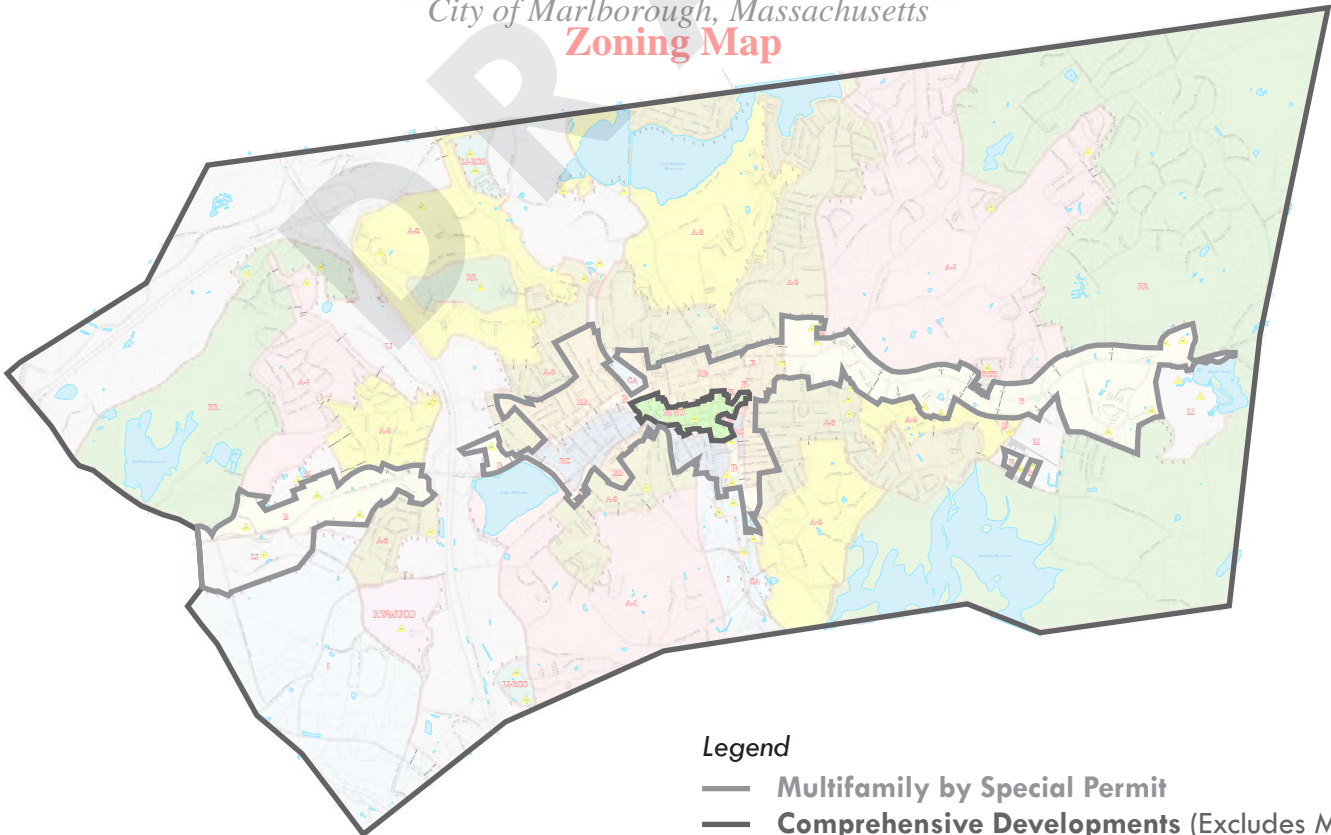
In the City of Marlborough, multifamily housing is allowed through two primary sections of the zoning ordinance - Multifamily by Special Permit and Comprehensive Developments. The applicable locations for these two approaches to multifamily housing are illustrated on the City of Marlborough Zoning Map below. Multifamily by Special Permit is allowed in the Marlborough Village District (MVD), Business Districts (B), Residence B (RB), and Residence C (RC) districts. Comprehensive Developments are allowed anywhere in the City, except the Marlborough Village District (MVD). The districts for the design guidelines respond to this regulatory context.

The three districts responding to varying context of the existing conditions of the City are established and described in more detail on the following pages. The design guidelines are divided into three sections based on these districts:

- Established Neighborhoods (EN)
- Commercial Corridors (CC)
- Commerce/Industrial Park (CIP)

As they relate to the zoning context, the EN and CC districts are most likely to occur in the Multifamily by Special Permit areas shown below. The CIP district is most likely to occur in the larger Comprehensive Developments areas shown below.

City of Marlborough, Massachusetts  
**Zoning Map**



## Design Guideline Districts

The Multifamily Design Guidelines (MDG) are based on a multiple district approach with design guidance that is specific to the characteristics of each district. Design review should occur based on the district that best fits for the context of the proposed multifamily development in the City. The determination of the districts is based on the *Multifamily Market and Fiscal Impact Analysis*' "Location Opportunities and Recommendations", the multifamily regulatory context of the zoning ordinance, and the characteristics of the existing housing patterns in the City.

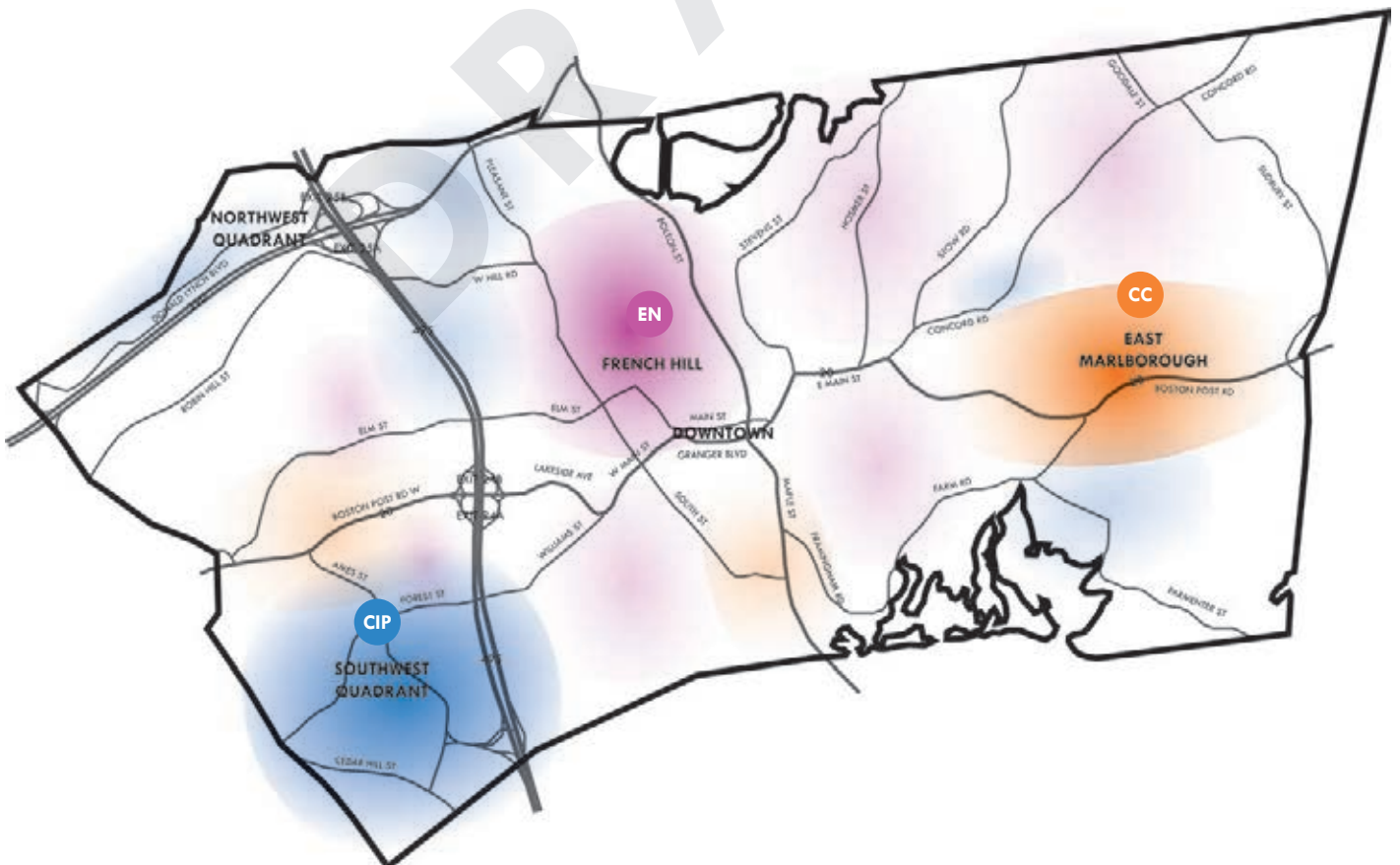
The ambition of a multiple district approach is to tailor more specific guidance to the specific needs of a particular area within the City and to provide more relevant information than could be provided with a "one size fits all" approach. More than (3) distinct districts could be identified across the City, but these three districts that are differentiated enough as to provide guidance for most relevant scenarios.

The Multifamily Design Guidelines districts include:

- **Established Neighborhood (EN)**
- **Commercial Corridor (CC)**
- **Commerce/Industrial Park (CIP)**

An example of the Established Neighborhood context is French Hill. An example of the Commercial Corridor context is East Marlborough along Route 20. An example of the Commerce/Industrial Park context is the Southwest Quadrant.

The *Multifamily Market and Fiscal Impact Analysis* also identifies Downtown Marlborough as a location opportunity. Downtown is the subject of *Design Review Guidelines for the Marlborough Village District*. The multifamily design guidance will be developed as a companion and complement to these Downtown Marlborough design guidelines drafted in 2014, but will not focus on it as a district.





A brief summary of each district context is based on in-person visits, review of recent documents, aerial photographs, and discussion with the Marlborough Economic Development Corporation (MEDC).

**EN Established Neighborhood** (example: French Hill)

**Approach:**

Smaller infill development to retain scale and character of traditional neighborhood with walkable streets

**Potentially Suitable Housing Types:**

Conversion of existing building, multiple units in house, townhouse, small multiple unit building

Example aerial:



Example photograph:



**CC Commercial Corridor** (example: East Marlborough)

**Approach:**

Moderate scale to reinforce walkable nodes and attractive corridor frontage

**Potentially Suitable Housing Types:**

Townhouse, small multiple unit building, large multiple unit building, multiple unit building over parking

Example aerial:



Example photograph:



**CIP Commerce/Industrial Park** (example: Southwest Quadrant)

**Approach:**

Large scale development integrating multiple housing types, amenities and open space to enhance walkability and bikability or incremental introduction of residential uses in long term transformation into walkable nodes

**Potentially Suitable Housing Types:**

Cluster of small houses, townhouse, small multiple unit building, large multiple unit building, multiple unit courtyard building, multiple unit building over parking, multiple unit building next to parking

Example aerial:



Example photograph:



**QUICK COMPARISON OF THE PRIMARY FOCUS FOR EACH CONTEXT:**

EN - Reinforcing traditional neighborhoods










CC - Strengthening corridor frontage and connected nodes

CIP - Creating walkable village-based clusters

### Potential Housing Types and Suitability Matrix

A check mark indicates a housing type that is potentially suitable for the district context listed depending on the specific location, property characteristics, and proposed multifamily development.

Potential Multifamily Housing Types:

		Established Neighborhood	Commercial Corridor	Commerce / Industrial Park
		EN	CC	CIP
<b>1 Conversion of Existing Building</b>		✓	✓	
<i>An existing, often historic, structure that may not have been a residential use, converted to use for residential</i>				
<b>2 Cluster of Small Houses</b>		✓		✓
<i>Modest buildings purposefully arranged around small open spaces</i>				
<b>3 Multiple Units in House</b>		✓		
<i>Multiple units in a larger structure typically accessed from a common entry and stair</i>				
<b>4 Townhouse</b>		✓		✓
<i>Units sharing side walls, may shared common entries or stairs, may be stacked on a garage</i>				
<b>5 Small Multiple Unit Building</b>			✓	✓
<i>Multiple units (4-9) served by a common entry and common interior corridor to access units in a small building</i>				
<b>6 Large Multiple Unit Building</b>			✓	✓
<i>Multiple units (10+) served by a common entry and common interior corridor to access units in a large building</i>				
<b>7 Multiple Unit Courtyard Building</b>				✓
<i>Multiple units served by a common entry and interior corridor that connect to form an interior courtyard</i>				
<b>8 Multiple Unit Over Parking</b>			✓	✓
<i>Multiple units served by a common entry and interior corridor that include parking in the building base</i>				
<b>9 Multiple Unit Next To Parking</b>				✓
<i>Multiple units arranged to conceal a parking structure</i>				



**MARLBOROUGH CITY-WIDE CONTEXT**

A general analysis of the City-wide context relative to the three design guidelines districts for multifamily development review.

ALL CALCULATIONS ARE APPROXIMATE ESTIMATES

NUMBER OF PARCELS: **10,560**

TOTAL LAND AREA: **14,208 acres**

AVERAGE PARCEL SIZE: **1.33 acres**

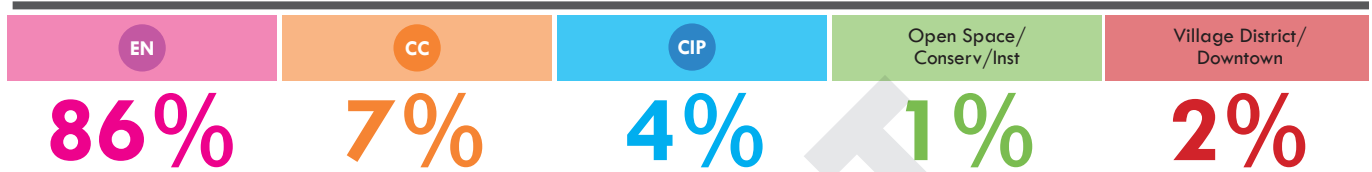
STREET ROW AREA: **1,339 acres** (Source: Marlborough Land Parcel Data 2012, most recent available)

TOTAL NUMBER OF RESIDENTIAL UNITS: **16,560** (Source: US Census ACS Estimate 2011-15)

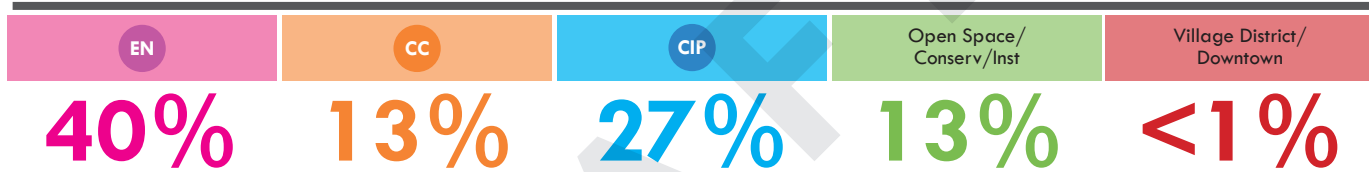
DENSITY OF UNITS: **1.2 units per acre**

**APPROXIMATE AREA OF APPLICABILITY FOR EACH DISTRICT CONTEXT**

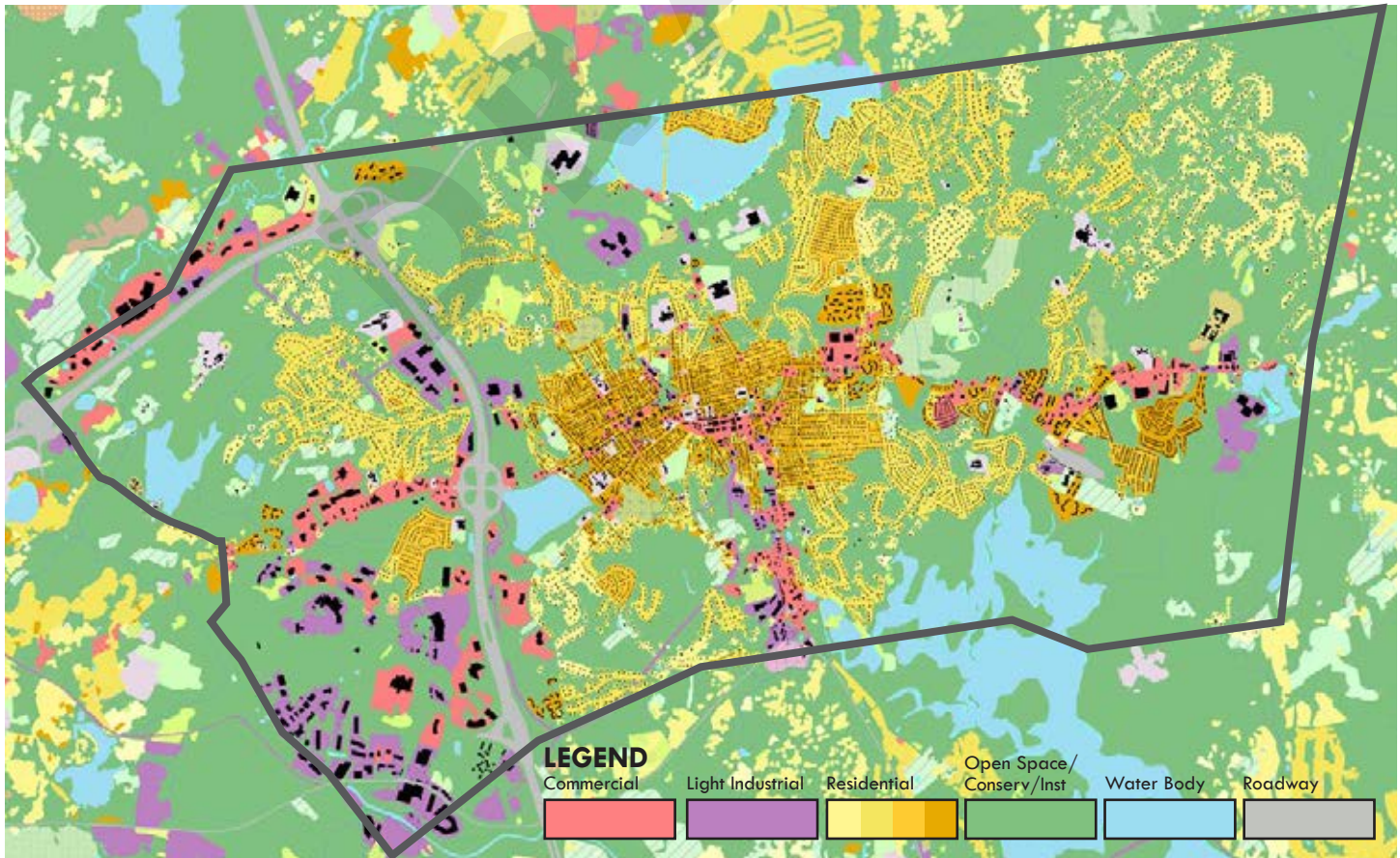
**% OF TOTAL PARCELS BY DISTRICT** (+/-1.5% MARGIN OF ERROR)



**% OF TOTAL LAND AREA BY DISTRICT** (BETWEEN 6-8% IS UNACCOUNTED, MAY BE WATER BODIES)



**CITY-WIDE LAND USE DIAGRAM**



# Marlborough Multifamily Design Guidelines

## EN ESTABLISHED NEIGHBORHOOD (EN) DISTRICT CONTEXT

### CONTEXT DESCRIPTION



***If the location of a multifamily housing proposal most closely matches this context description, then this section of the Multifamily Design Guidelines should be used for the project review.***

**GENERAL CHARACTER:** The “Established Neighborhood” context is characterized by large and modest single-family and multifamily residential buildings, interspersed with smaller commercial or institutional uses. These uses are organized with a traditional street and block grid with sidewalks and mature trees.

**STREET AND BLOCK PATTERNS:** Most of these areas are set apart from the major roadway connections in the City. Most blocks are of a walkable traditional neighborhood scale with a network of connecting streets. Some areas are more suburban with winding streets that connect less frequently and end in a cul-de-sac.

**BUILDING PLACEMENT AND LOCATION:** Buildings are oriented to the street typically set back behind a front yard.

**BUILDING HEIGHT:** A height of two-stories is most prominent with variation including one-story and three-story buildings occasionally.

**MOBILITY:** Walking, biking, and driving are the primary forms of transportation in these areas.



EN

## ESTABLISHED NEIGHBORHOOD (EN)

### CONTEXT APPLICABILITY

**NUMBER OF PARCELS:** 9,020 (approximately)

**TOTAL LAND AREA:** 5,604 acres (approximately)

**AVERAGE PARCEL SIZE:** 0.62 acres (approximately)

**EXAMPLE:** FRENCH HILL

### APPROACH:

Smaller infill development to retain scale and character of traditional neighborhood with walkable streets

**FOCUS:** Reinforcing traditional neighborhoods

### CONTEXT SUITABILITY

POTENTIALLY SUITABLE HOUSING TYPES:



Conversion of Existing Building



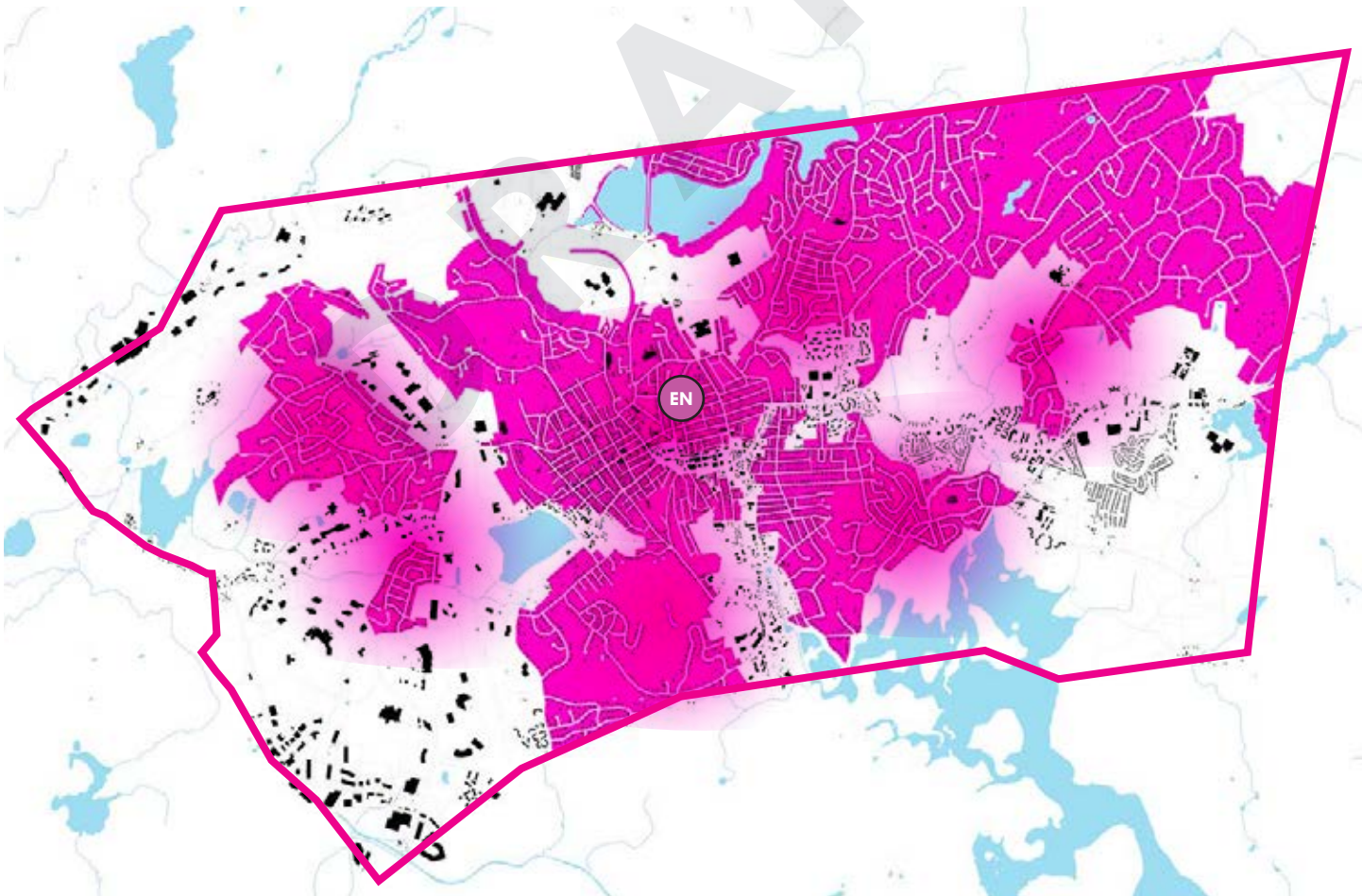
Multiple Units in House



Townhouse



Small Multiple Unit Building



### MAP OF CONTEXT APPLICABILITY:

Generalized boundaries of where this type of design guidance may be applicable in the City



EN

## ESTABLISHED NEIGHBORHOOD (EN)

### SITE DESIGN GUIDELINES

#### Context Sensitive

Relate the site layout patterns of abutting properties to harmonize with street frontage conditions and strengthen the sense of community on the block



**1 Setbacks** - Respect abutting setbacks to reinforce established norms of the placement of buildings and treatment of frontages on the site. Provide landscape and buffers at setbacks to adjacent properties to conceal parking, loading and service areas

**2 Orientation of Building** - Design the site and building to define the street frontage of the property through the placement and orientation of the building on the site with the primary facade facing the street and site circulation engaging the street

**3 Transitions and Buffers** - Reinforce sensitivity to the scale of neighboring buildings by stepping down building massing near property edges. Buffers to adjacent property edges should include landscape, trees, fencing or other screening methods

**4 Open Spaces or Plazas** - Site open space and plazas should be located and positioned to expand existing and adjacent amenities to allow for continuous visual and physical connections to existing open spaces or plazas. Mature trees on site should be preserved

## ESTABLISHED NEIGHBORHOOD (EN)

### SITE DESIGN GUIDELINES

#### Site Configuration

Each design guideline in this category offers techniques to highlight the positive aspects of a well design site and configuration of buildings that fit in with the surrounding neighborhood while minimizing impacts on adjacent properties.

5



**5 Positioning of Building** - The building should be the prominent feature of the site and should be placed nearly centered on the property side-to-side and biased to the front of the property. The building should frame street frontage and open spaces

6



**6 Driveways** - Curb cuts and site vehicular access should be minimized in frequency and width and should be combined with adjacent properties when the opportunity exists. Vehicular access should not dominate the site plan or the property and street frontage

7



**7 Location of Parking** - Design the site to place all parking areas to the rear of buildings. Minimize the visual impact of parking with landscaping and screening, and position buildings to more directly contribute to the character and pedestrian orientation of the district

8



**8 Location of Service, Loading and Utility Areas** - Design the site layout to minimize impact, visibility, and public view of service areas, loading docks, dumpsters, utilities, or other utilitarian functions. Screen these components with landscaping and fencing

9



**9 Sustainable Site Layout** - Design the site and building layout to benefit from an effective solar orientation for passive heating and cooling and use of renewable energy sources on site

## ESTABLISHED NEIGHBORHOOD (EN)

### SITE DESIGN GUIDELINES

#### **Parking and Circulation**

*Parking and vehicular circulation in an established neighborhood should be minimized and visually concealed with pedestrian circulation highlighted and strengthened to enhance the quality of life and sense of community in the neighborhood*



10

**10 Walkways** - Pedestrian access should be provided to the building entries and parking areas connecting to the sidewalk at the street frontage, pedestrian connections should be provided to adjacent amenities, paths or trails, or other connections to adjacent properties



11

**11 Vehicular Circulation** - Circulation in parking areas should be designed to allow for connections to existing parking areas on adjacent properties, internal circulation should be designed to allow for the convenient and efficient shared use of parking between properties



12

**12 Alternative Modes of Travel** - Convenient locations for bicycle parking should be provided and locations for car share spaces in the parking lot should be considered. If a bus stop is present additional frontage area should be considered for amenities



13

**13 Site Furnishings** - Seating, benches, trash receptacles, bike racks, and screening elements should be coordinated with consistent materials and appropriate locations to provide an integrated approach to amenities



14

**14 Site Lighting** - Lighting should be of a pedestrian-scale and focused on safe lighting levels for use of the property while avoiding light spill onto adjacent properties and light impact on the night sky



EN

## ESTABLISHED NEIGHBORHOOD (EN)

### SITE DESIGN GUIDELINES

#### External Materials and Landscape

Each design guideline in this category reflects the importance of details to strengthen the quality of the project and elevate the design to support the neighborhood and reinforce community

15



**15 Quality Materials** - Selection of external site materials should focus on quality, durability, and sustainability with materials that elevate the quality of the context and should include granite curbs, concrete sidewalks, and accent features such as brick or stone pavers

16



**16 Welcoming Streetscape** - The street frontage(s) should integrate a thoughtful landscape that is appropriate to the context of the building and surrounding streetscape. Public and private investments should be integrated to provide consistent street trees and design

17



**17 Landscape Features** - Integrate planting and site features through the use of window boxes, entry or seating area trellises, vertical gardens and green walls on blank facades, and foundation plantings to anchor buildings to the site.

18



**18 Landscape** - Trees and other plantings should be used to define outdoor spaces and screen negative views while enhancing the sense of community. Plantings should be species native to Eastern Massachusetts, long-lived and hardy, and include regular shade trees

19



**19 Sustainable Site Features** - Integrate low impact development techniques and sustainable features such as rain gardens, cisterns, porous pavers, or stormwater management features into the site design

EN

## ESTABLISHED NEIGHBORHOOD (EN)

### BUILDING DESIGN GUIDELINES

#### Context Sensitive

Each design guideline in this category offers techniques to strengthen context sensitivity of the building. In an established neighborhood, the context is specific and the proposed design should respond to the context in a way that elevates the positive aspects of the surrounding community.



**20 Orientation of Building** - The building should be designed to face the primary street of the property frontage, this orientation is achieved through the layout of the plan, design of the building form, and location of building entries and lobby

**21 Transitions and Buffers** - The building should step down in height, or reduce the volume of roof form adjacent to an existing building of a lower height

**22 Complementary Building Forms** - The layout of the building plan and design of building massing should complement adjacent structures by providing a similar scale at the street frontage

**23 Respect the Neighboring Context** - Relate the style and form of the building to abutting properties to complement the neighborhood context.



EN

## ESTABLISHED NEIGHBORHOOD (EN)

### BUILDING DESIGN GUIDELINES

#### **Building Configuration**

Each design guideline in this category offers techniques to create high quality buildings that fit into the neighborhood and strengthen the surrounding context to create high quality places.

24



24

**Height** - Within the zoning limitations on height, further reductions in height should be used to respond to the surrounding context near property edges

25



25

**Scale** - The scale of a building should be biased toward the portion of the site least visible from the street frontage with the intention of allowing larger scale structures that fit into the context

26



26

**Roof Form** - The roof form should be used to reduce the overall scale of large structures, add visual interest to the building, and complement the immediate context of structures

27



27

**Define Facade with Detail** - Reinforce district architectural patterns with understated and simple facade details that are constructed of high quality materials designed to draw attention to doors, windows, ground floor levels, cornices, and eaves.

## ESTABLISHED NEIGHBORHOOD (EN)

### BUILDING DESIGN GUIDELINES

#### Facade and Appearance

Each design guideline in this category offers techniques to create visually interesting buildings that are consistent with the context through the design of the building's faces or facades.

28



28

**Entrances** - The primary building entry should be a feature of the building facade and be anchored by the building massing; avoid the appearance of the entry “tacked on” to the building

29



29

**Garage Doors** - Garage doors should not be the prominent feature of the front building facade, placement of garages should be on the rear or side of the building

30



30

**Windows** - Windows should be used as a primary feature of facades to provide a sense of scale and relate to the surrounding building context through window size, pattern, and spacing. Facade detail and trim should be used to frame and define window locations

31



31

**Horizontal Definition** - The building facade should be composed of several horizontal bays to form a visually distinct pattern that reduces the overall scale of the structure; avoid complete repetition across a flat facade, provide variety in the design and depth of these bays

32



32

**Vertical Definition** - The building facade of a large scale structure should also relate to the surrounding context by differentiating materials of the facade vertically, a base material may relate to an adjacent single-story structure or an upper-story may be a different material

EN

## ESTABLISHED NEIGHBORHOOD (EN)

### BUILDING DESIGN GUIDELINES

#### **Additional Considerations**

Each design guideline in this category offers additional approaches to elevating the quality of the multifamily development in an existing neighborhood.

33



33

**Quality Materials** - Exterior building materials should be high quality, durable, and sustainable and avoid materials not consistent with the context such as stucco products

34



34

**Sustainable Design** - The integration of sustainable design approaches and features into the building are encouraged including participating in a sustainability guidance and rating system such as Leadership in Energy and Environmental Design (LEED green buildings)

35



35

**Historic Structures** - Integrate into the design with renovation and additions that are complementary to the historic structure and consistent with the Secretary of the Interior's Standards for Rehabilitation. If not feasible, relocation of the structure should be evaluated

36



36

**Signage** - If signage is required for the property, it should be minimized and designed to be consistent with address numbers and to integrate with the design of the building facade



# Marlborough Multifamily Design Guidelines

## CC COMMERCIAL CORRIDOR (CC) DISTRICT CONTEXT

### CONTEXT DESCRIPTION



***If the location of a multifamily housing proposal most closely matches this context description, then this section of the Multifamily Design Guidelines should be used for the project review.***

**GENERAL CHARACTER:** The “Commercial Corridor” is characterized by the presence of a major roadway in the City and frequent commercial uses. The residential context includes multifamily residential buildings set within the larger commercial context.

**STREET AND BLOCK PATTERNS:** The streets and blocks are oriented to the primary roadway (State Routes 20 and 85).

**BUILDING PLACEMENT AND LOCATION:** Buildings are often placed setback from the commercial corridor with parking in between the building and roadway.

**BUILDING HEIGHT:** Multifamily residential buildings vary from 2-story to 4-story, most commercial and retail buildings are one-story.

**MOBILITY:** The pattern is distinctly auto-oriented, pedestrian activity is constrained by automobile circulation and distances to be traveled.



**CC COMMERCIAL CORRIDOR (CC)**

**CONTEXT APPLICABILITY**

**NUMBER OF PARCELS:** 700 (approximately)  
**TOTAL LAND AREA:** 1,756 acres (approximately)  
**AVERAGE PARCEL SIZE:** 2.5 acres (approximately)  
**EXAMPLE:** EAST MARLBOROUGH

**APPROACH:**  
Moderate scale to reinforce walkable nodes and attractive corridor frontage

**FOCUS:** Strengthening corridor frontage and connected nodes

**CONTEXT SUITABILITY**

POTENTIALLY SUITABLE HOUSING TYPES:



Townhouse



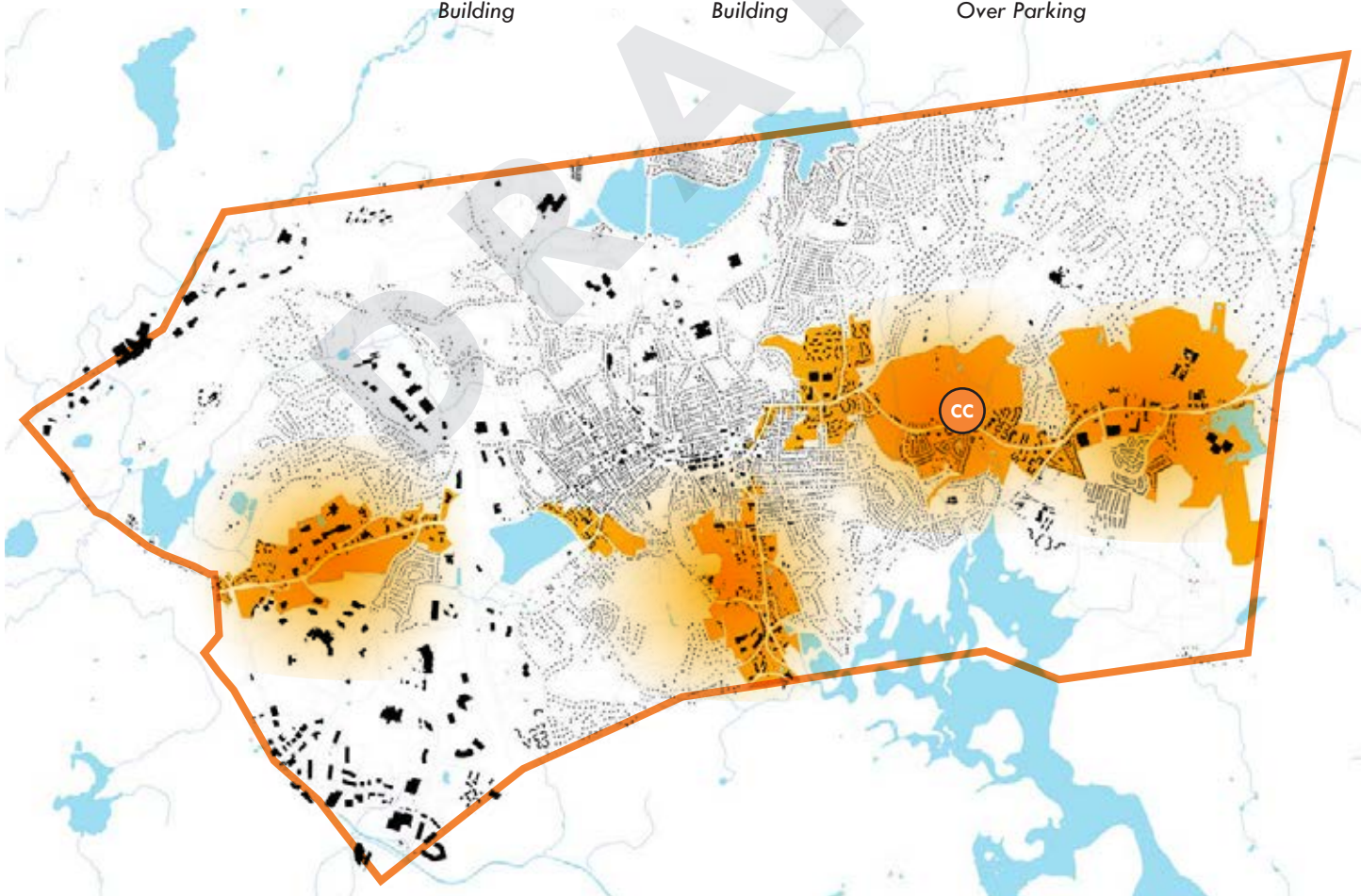
Small Multiple Unit Building



Large Multiple Unit Building



Multiple Unit Building Over Parking



**MAP OF CONTEXT APPLICABILITY:**

Generalized boundaries of where this type of design guidance may be applicable in the City

**CC COMMERCIAL CORRIDOR (CC)**

**SITE DESIGN GUIDELINES**

**Context Sensitive**

Each design guideline in this category offers techniques to strengthen context sensitivity of the site design. The site plan and new building positions should enhance the pattern of the site and strengthen the sense of place by relating to surrounding patterns.



**1 Site Organization** - Design site and arrange buildings and site circulation to strengthen the sense of a “public realm” by framing and defining open space, street frontages, and amenities while creating relationships with the surrounding context

**2 Setbacks** - A setback from the commercial corridor should be used to buffer housing from higher volume traffic at street frontage with a landscape buffer and should connect multi-use paths along the property frontage with circulation into the site

**3 Transitions and Buffers** - The layout of the site and building(s) should provide a transition in scale to adjacent residential context, with particular concern for abutting single family homes. A robust landscape buffer near these property lines should be integrated into the site.

**4 Open Spaces or Plazas** - A network of modest open spaces and plazas should be a carefully considered part of the site design connecting building entries and common amenities while adding community connections that may link with adjacent properties’ open spaces

**CC COMMERCIAL CORRIDOR (CC)**

**SITE DESIGN GUIDELINES**

**Site Configuration**

Each design guideline in this category offers techniques to highlight the positive aspects of a well design site and configuration of buildings while minimizing impacts on adjacent properties.



5 **Compact Design** - Enhance sustainability and reduce development footprint by clustering buildings and site development to minimize the impacts and protect natural features of the site. Cluster buildings to concentrate framing of open space and creation of a sense of place



6 **Positioning of Building** - The building(s) should be positioned on the site to maximize a sense of community and the creation of a place by framing street frontages, open spaces, and defining relationships between buildings and the surrounding context



7 **Location of Parking** - Parking should be integrated into the overall site design to reduce its visual impact by placing it to the side or rear of buildings, breaking large parking lots into several smaller parking lots and integrating parking with the open space and landscape design



8 **Location of Service, Loading and Utility Areas** - Service, loading and utility areas should be integrated with the overall site design and reduced by locating these areas to the rear of buildings and screening views from the commercial corridor and abutting properties



9 **Strengthening of Amenities** - Design site circulation to strengthen connections to parks, lakefront trails, rail trails or other amenities that connect to or that are nearby the property. Expand positive amenities with new small plazas, sitting areas, landscape, or public art



**CC COMMERCIAL CORRIDOR (CC)**

**SITE DESIGN GUIDELINES**

**Parking and Circulation**

The design of circulation and parking should be an integral part of connecting to the context, strengthening walkability by buffering pedestrians from vehicular traffic with landscaping buffers, and enhancing the quality of life in the commercial corridor.

10



10

**Driveways** - A central and primary driveway should be designed to provide safe and convenient access including turn lanes, acceleration/deceleration lanes or other transitions from the commercial corridor. Reduce the frequency and width of curb cuts that interrupt sidewalks.

11



11

**Walkways** - Pedestrian circulation should be prioritized in the site plan providing continuous connection along the commercial corridor frontage, along internal accessways, connecting to each building entry, common amenity, and open space, with marked crosswalks

12



12

**Crosswalks** - Walkability should be enhanced through well-marked street and driveway crossings with curb extensions to reduce the street crossing distance for pedestrians and reduce vehicular speeds at all intersections, mid-block crossings, and curb-cuts.

13



13

**Minimize Parking** - Use on-street parking internal to the site to add to parking supply while reducing large surface parking lots and place parking lots to the rear and the side of building, avoiding primary street frontages and integrating parking with landscape

14



14

**Alternative Modes of Travel** - Primary walkways/circulation routes should be wide enough to be used as multi-use paths for bicycles. If a bus stop is present, a dedicated and sheltered area should be provided in a central location, spaces for car share services should be provided



**CC COMMERCIAL CORRIDOR (CC)**

**SITE DESIGN GUIDELINES**

**External Materials and Landscape**

Each design guideline in this category reflects the importance of details to strengthen the quality of the project and elevate the design.

15



15

**Quality Materials** - Selection of external site materials should focus on quality, durability, and sustainability with materials that elevate the quality of the context and should include granite curbs, concrete sidewalks, and accent features such as brick or stone pavers

16



16

**Welcoming Streetscape** - The street frontage(s) should integrate a thoughtful landscape that is appropriate to the context of the building and surrounding streetscape. Public and private investments should be integrated to provide consistent street trees and design

17



17

**Landscape** - Trees and other plantings should be used to define outdoor spaces and screen negative views while enhancing the sense of community. Plantings should be species native to Eastern Massachusetts, long-lived and hardy, and include regular shade trees

18



18

**Site Furnishings** - Seating, benches, trash receptacles, bike racks, and screening elements should be coordinated with consistent materials and appropriate locations throughout the site to add amenity and functionality

19



19

**Site Lighting** - Lighting should be of a pedestrian-scale and focused on safe lighting levels for use of the property while avoiding light spill onto adjacent properties and light impact on the night sky. Use lighting of landscape and site features to enhance features

**CC COMMERCIAL CORRIDOR (CC)**

**BUILDING DESIGN GUIDELINES**

**Context Sensitive**

The building context may vary, including adjacent commercial or residential uses of varying scales, the building design should respond to the context of the property to create a thoughtful contribution that will strengthen a walkable node with a sense of place.

20



20

**Orientation of Building** - Building(s) should be designed to define the commercial corridor frontage and frame the frontage of streets internal to the site. Buildings at the frontage should achieve both, with buildings interior to the site framing interior streets and open spaces

21



21

**Transitions and Buffers** - Building massing and scale should step down to lower scale neighbors, particularly single family homes which may abut the property. Transitions and buffers should be used as fundamental principles in the design and layout of site and buildings

22



22

**Complementary Building Forms** - Complement adjacent structures, create compatibility across multiple structures or building types, while avoiding repetition of the same type uniformly. This may be achieved through variation in massing, style, roof form, or facade design

23



23

**Height** - Variation in building height should be used strategically to add visual interest through perspective, depth, and overlapping forms. Larger building heights should be buffered by lower heights that provide a layered approach to build up height and respect context

24



24

**Mixed-use** - Upper levels of multifamily should be integrated with lower levels of retail and office uses that provide active ground floor uses oriented to the primary frontage of the commercial corridor.

**CC COMMERCIAL CORRIDOR (CC)**

**BUILDING DESIGN GUIDELINES**

**Building Configuration**

Each design guideline in this category offers techniques to create visually interesting buildings that fit together with the surrounding context to create high quality places.

25



25

**Scale** - Reduce the overall scale of a building and collection of buildings by placing lower volumes in front of larger volumes, stepping back the building massing, at the top floor(s) and relating to the human scale through details

26



26

**Massing** - The organization of the building's overall volume shall be used to reduce the impression of scale and provide visual interest. Create variations in the building floor plan, and elements of the building volume with recesses, projections, or setbacks

27



27

**Roof Form** - The roof form should be consistent with architectural style of the building and provide an opportunity to add visual interest to the building massing while reducing the perception of the overall building scale

28



28

**Roof Transitions** - Building massing and the facade design should not just end at the top, but should be integrated into the design of the roof, whether that roof is flat or pitched with a cornice, parapet, or other design feature



**CC COMMERCIAL CORRIDOR (CC)**

**BUILDING DESIGN GUIDELINES**

**Facade and Appearance**

Each design guideline in this category offers techniques to create visually interesting buildings through the design of the building's faces or facades.



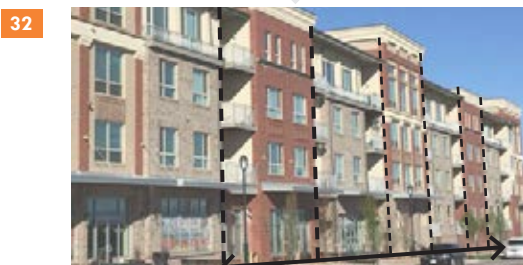
**29 Entrances** - Building entries should be designed as a feature of the facade that should be highlighted. The building massing, facade design, and site design should all respond to the location of building entries and reinforce the human scale of the building



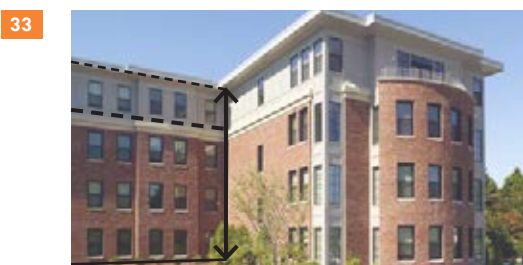
**30 Garage Doors** - Garages and garage doors should be located on secondary facades and designed to minimize their visual impact and prominence as part of the design of the building facade, as well as the overall building and site design



**31 Windows** - Window composition on the facade should create a rhythm, sense of scale, and overall consistency with the design. Windows should be proportioned to emphasize the vertical, with a width to height ratio of no less than 1 to 2



**32 Horizontal Definition** - The building facade should be composed of several horizontal bays not more than 50 feet in width to reduce the overall scale of the structure. Provide variety in the design, depth, and materials of the bays and avoid repetitious patterns



**33 Vertical Definition** - The perceived scale of a building can be reduced by differentiating the base, middle, and top of the facade with variation in building massing and building materials. This definition may not apply uniformly across the building facades



**CC COMMERCIAL CORRIDOR (CC)**

**BUILDING DESIGN GUIDELINES**

**Additional Considerations**

Each design guideline in this category offers additional approaches to elevating the quality of the multifamily development.



**34 Balconies and Roof Decks** - Use balconies and roof decks to capitalize on surrounding site views while also providing an element of visual interest for the facade. Balconies should project to create a sense of depth and variety for the facade



**35 Quality Materials** - Exterior building materials should be high quality, durable, sustainable, natural and age gracefully. Multiple materials should be selected that are compatible and harmonious. Examples include combinations of brick, stone, metal panels, or wood cladding



**36 Sustainable Design** - The integration of sustainable design approaches and features into the building are encouraged including participating in a sustainability guidance and rating system such as Leadership in Energy and Environmental Design (LEED)



**37 Historic Structures** - Historic structures should be integrated into the redevelopment design with renovation and additions that are complementary to the historic structure and consistent with the Secretary of the Interior's Standards for Rehabilitation



**38 Signage** - If signage is required for the property, it should be minimized and designed to be consistent across the property and coordinated with address numbers and to integrated with the design of the building facades

# Marlborough Multifamily Design Guidelines

## CIP COMMERCE/INDUSTRIAL PARK (CIP) DISTRICT CONTEXT

### CONTEXT DESCRIPTION



***If the location of a multifamily housing proposal most closely matches this context description, then this section of the Multifamily Design Guidelines should be used for the project review.***

**GENERAL CHARACTER:** The “Commerce/Industrial Park” is characterized by large properties of predominantly commercial or light industrial uses arranged with access drives and large parking areas set within wooded areas of the City. It may also include parcels or portions of parcels that are largely undeveloped, typically with mature tree cover for most of the lot.

**STREET AND BLOCK PATTERNS:** Sites are designed for internal circulation and result in a disconnected pattern of streets where circulation is only possible by automobile.

**BUILDING PLACEMENT AND LOCATION:** Buildings are arranged around an internal logic of the design of the property, little regard is given to the surrounding context.

**BUILDING HEIGHT:** Buildings range from 1-story to 5-story.

**MOBILITY:** The scale of the properties and the distance between destinations reduces the viability of non-auto modes of travel.



**CIP** **COMMERCE/INDUSTRIAL PARK (CIP)**

**CONTEXT APPLICABILITY**

**NUMBER OF PARCELS:** 418 (approximately)  
**TOTAL LAND AREA:** 3,720 acres (approximately)  
**AVERAGE PARCEL SIZE:** 8.90 acres (approximately)  
**EXAMPLE:** **SOUTHWEST QUADRANT**

**APPROACH:**

Large scale development integrating multiple housing types, amenities and open space to enhance walkability and bikability, or incremental introduction of residential uses in a long term transformation into walkable nodes, or “villages”

**FOCUS:** **Creating walkable village-based clusters**

**CONTEXT SUITABILITY**

POTENTIALLY SUITABLE HOUSING TYPES:



Cluster of Small Houses

Townhouse

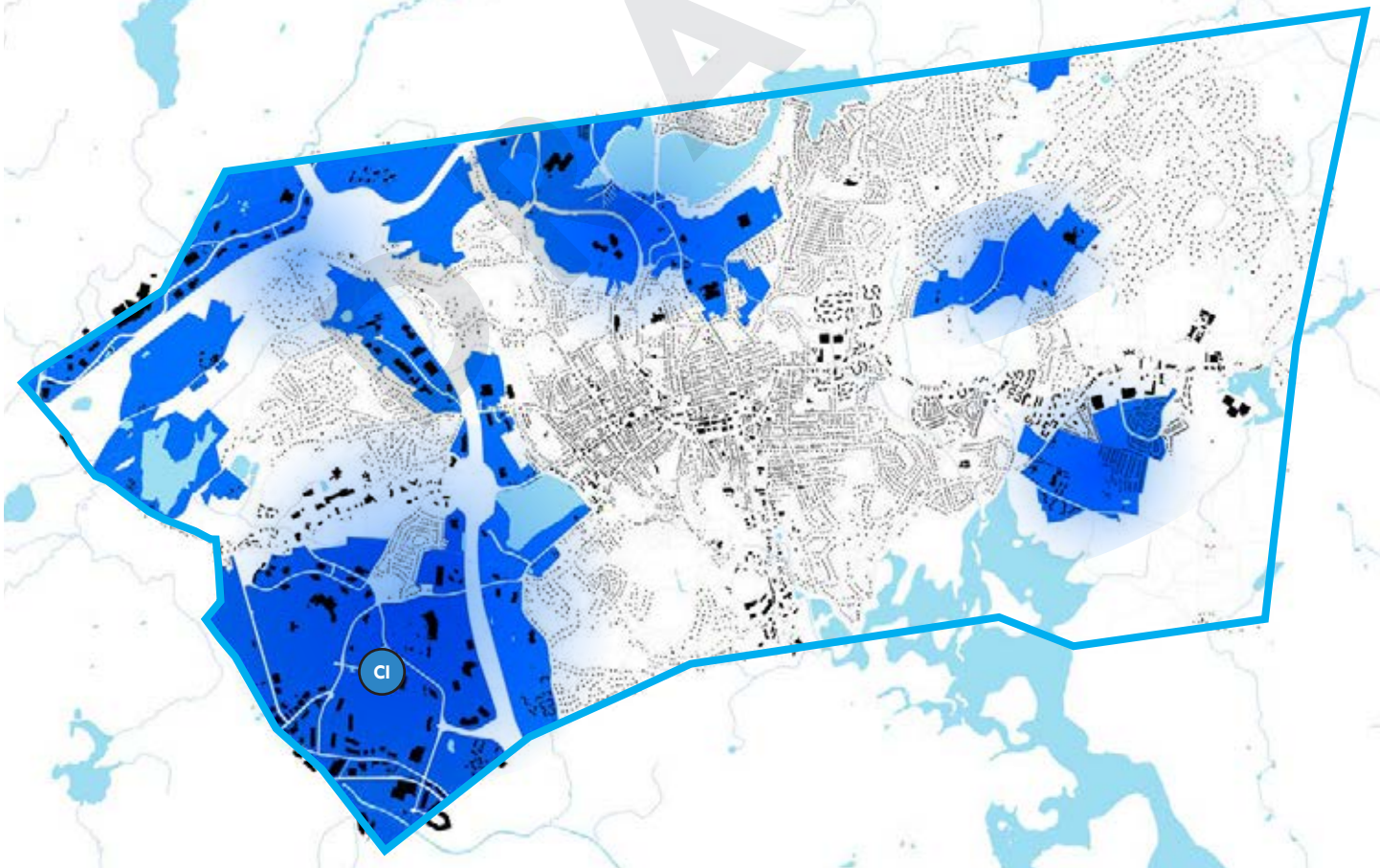
Small Multiple Unit Building

Large Multiple Unit Building

Multiple Unit Courtyard Building

Multiple Unit Building Over Parking

Multiple Unit Next to Parking



**MAP OF CONTEXT APPLICABILITY:**

Generalized boundaries of where this type of design guidance may be applicable in the City



## COMMERCE/INDUSTRIAL PARK (CIP)

### SITE DESIGN GUIDELINES

#### Context Sensitive

Each design guideline in this category offers techniques to strengthen context sensitivity of the site design. If existing buildings are present, the site plan and new building positions should enhance the pattern of the site and strengthen the sense of place.

1



**1 Site Organization** - Design site and arrange buildings and site circulation to strengthen the sense of a “public realm” by framing and defining open space, street frontages, and amenities while showcasing the natural landscape

2



**2 Setbacks** - Retain setbacks of natural tree buffers at the edges of the property to showcase the natural landscape and conceal the development with the natural buffer. The site layout should connect to the natural amenities of the site and preserve natural features

3



**3 Orientation of Building** - Create a site design in which buildings have a clear relationship to the streets and define open spaces. Building orientation should reinforce site circulation patterns, open space patterns, and connections to other buildings on site

4



**4 Transitions and Buffers** - Use natural topography and site features to provide visual buffers and landmarks to focus the development around. Place lower scale development near the perimeter and site entries to transition to context

5



**5 Open Spaces or Plazas** - Respond to the natural features of the site to provide usable open space, plazas should be integrated with the buildings and site circulation to provide common outdoor amenities and gathering places

## COMMERCE/INDUSTRIAL PARK (CIP)

### SITE DESIGN GUIDELINES

#### Site Configuration

Each design guideline in this category offers techniques to highlight the positive aspects of a compact cluster of buildings while minimizing impacts of functional aspects of the site.

6



6

**Compact Design** - Enhance sustainability and reduce development footprint by clustering buildings and site development to minimize the impacts and protect natural features of the site

7



7

**Positioning of Building** - Buildings should be configured to respond to the site, optimize views, define outdoor spaces, and engage with the circulation network. Buildings should be organized in compact and efficient clusters to define common spaces and conserve natural land

8



8

**Location of Parking** - Parking should be integrated into the overall site design to minimize impact, reduce the loss of trees, and to be visually concealed. Topography of the site should be used to conceal parking with berms, retaining walls, and parking below buildings

9



9

**Location of Service, Loading and Utility Areas** - Should be integrated with the overall site design to minimize impact, reduce the loss of trees, and to be visually concealed. Topography and landscape features should conceal these areas placed to the rear of buildings

10



10

**Strengthening of Amenities** - Design site circulation to strengthen connections to parks, lakefront trails, rail trails or other amenities that connect to or that are nearby the property. Expand positive amenities with new small plazas, sitting areas, landscape, or public art



## COMMERCE/INDUSTRIAL PARK (CIP)

### SITE DESIGN GUIDELINES

#### **Parking and Circulation**

The design of circulation and parking should be an integral part of strengthening the site configuration to elevate design quality and reinforce walkability.

11



**11 Streets and Driveways** - Internal site circulation should be used to organize the site and provide multiple routes and connections internal to the site design and external to surrounding street network. A high quality “public realm” with a street network is an important site feature

12



**12 Walkways** - Attractive and convenient pedestrian circulation should be prioritized in the site plan to provide continuous connections along internal streets, to each building entry, common amenity, and open space, and combine with marked crosswalks

13



**13 Vehicular Circulation** - In addition to access to the surrounding street network and internal parking areas, vehicular circulation should provide a pick-up/drop-off area, sheltered waiting area, and wayfinding within the site

14



**14 Minimize Parking** - Use on-street parking to add to parking supply while reducing large surface parking lots and use parking reserve areas to identify potential areas for future phases of parking that would meet total parking requirements that may not be needed

15



**15 Alternative Modes of Travel** - Primary walkways/circulation routes should be wide enough to be used as multi-use paths for bicycles. A dedicated and sheltered area for a shuttle bus service should be provided in a central location



## COMMERCE/INDUSTRIAL PARK (CIP)

### SITE DESIGN GUIDELINES

#### **External Materials and Landscape**

Each design guideline in this category reflects the importance of details to strengthen the quality of the project and elevate the design.

16



**16 Quality Materials** - Selection of external site materials should focus on quality, durability, and sustainability with materials that elevate the quality of the context and should include granite curbs, concrete sidewalks, and accent features such as brick or stone pavers

17



**17 Landscape** - Trees and other plantings should be used to define private and public spaces while enhancing the sense of community. Plantings should be species native to Eastern Massachusetts, long-lived and hardy, and include shade trees in the site design

18



**18 Sustainable Design** - Integrate low impact development techniques and sustainable stormwater management features into the site design while maintaining a compact development footprint

19



**19 Site Furnishings** - Seating, benches, trash receptacles, bike racks, and screening elements should be coordinated with consistent materials and appropriate locations throughout the site design

20



**20 Site Lighting** - Lighting should be pedestrian-scale, at an appropriate level for safety, color-corrected with a preference for LED fixtures focused on reducing glare and avoiding light impact on adjacent properties and the night sky. Avoid too much light and floodlighting

## COMMERCE/INDUSTRIAL PARK (CIP)

### BUILDING DESIGN GUIDELINES

#### Context Sensitive

The building context may vary widely, from commercial uses to a greenfield property, the building design should respond to the context of the property to create a thoughtful and unified whole that is composed of varied and complementary parts.

21



**21 Orientation of Building** - Building(s) should be designed to strengthen an internal logic for the site to define frontage of internal streets and open spaces. Building entries and fronts should be placed to activate the adjacent space

22



**22 Transitions and Buffers** - Transitions in building massing should be used to strengthen views and reduce building scale near property buffers to reduce the visual impact of the development, particularly near single family homes which may abut the property

23



**23 Complementary Building Forms** - Complement adjacent structures and create compatibility across multiple structures or building types while avoiding repetition of the same type uniformly. Create a consistent design vocabulary that is applied with invention

24



**24 Height** - Variation in building height should be used strategically within the development to add visual interest through perspective, depth, and overlapping forms to reduce repetition, and optimize views within the development

## COMMERCE/INDUSTRIAL PARK (CIP)

### BUILDING DESIGN GUIDELINES

#### **Building Configuration**

Each design guideline in this category offers techniques to create visually interesting buildings that fit together to create high quality places.

25



25

**Scale** - Reduce the overall scale of a building and collection of buildings by placing lower volumes in front of larger volumes, stepping back the building massing, at the top floor(s) and relating to the human scale through details

26



26

**Massing** - The organization of the building's overall volume shall be used to reduce the impression of scale and provide visual interest. Create variations in the building floor plan, and elements of the building volume with recesses, projections, or setbacks

27



27

**Roof Form** - The roof form should be consistent with architectural style of the building and provide an opportunity to add visual interest to the building massing while reducing the perception of the overall building scale

28



28

**Roof Transitions** - Building massing and the facade design should not just end at the top, but should be integrated into the design of the roof, whether that roof is flat or pitched with a cornice, parapet, or other design feature



## COMMERCE/INDUSTRIAL PARK (CIP)

### BUILDING DESIGN GUIDELINES

#### Facade and Appearance

Each design guideline in this category offers techniques to create visually interesting buildings through the design of the building's faces or facades.

29



29

**Entrances** - The building massing, facade design, and site design should all respond to the location of building entries which should be highlighted as a feature of the facade while providing shelter from weather and features to reinforce the human scale of the building

30



30

**Garage Doors** - Garages and garage doors should be located on secondary facades and designed to minimize their visual impact and prominence as part of the design of the building facade, as well as the overall building and site design

31



31

**Windows** - Window composition on the facade should create a rhythm, sense of scale, and overall consistency with the design. Windows should be proportioned to emphasize the vertical, with a width to height ratio of 1 to 2. Multiple windows should be combined for a wider expanse

32



32

**Horizontal Definition** - The building facade should be composed of several horizontal bays not more than 50 feet in width to reduce the overall scale of the structure. Provide variety in the design, depth, and materials of the bays and avoid repetitious patterns

33



33

**Vertical Definition** - The perceived scale of a building can be reduced by differentiating the base, middle, and top of the facade with variation in building massing and building materials. This definition may not apply uniformly across the building facades

## COMMERCE/INDUSTRIAL PARK (CIP)

### BUILDING DESIGN GUIDELINES

#### **Additional Considerations**

Each design guideline in this category offers additional approaches to elevating the quality of the multifamily development.

34



34

**Balconies and Roof Decks** - Use balconies and roof decks to capitalize on surrounding site views or views of natural amenities while also providing an element of visual interest for the facade. Balconies should project to create a sense of depth and variety for the facade.

35



35

**Quality Materials** - Exterior building materials should be high quality, durable, sustainable, natural and age gracefully. Multiple materials should be selected that are compatible and harmonious. Examples include combinations of brick, stone, metal panels, or wood cladding

36



36

**Sustainable Design** - The integration of sustainable design approaches and features are encouraged including energy efficiency, renewable energy, water efficiency and management, sustainable materials, indoor air quality or other areas of innovation

37



37

**Historic Structures** - If the property includes historic structures, the structures should be integrated into the redevelopment design with renovation and additions that are complementary to the historic structure and consistent with Secretary of the Interior's Standards

38



38

**Signage** - If signage is required for the property, it should be minimized and designed to be consistent across the property and coordinated with address numbers and integrated with the design of the building facades



04/26/2018  
DRAFT FOR UAC DISCUSSION

DRAFT



Marlborough Multifamily  
Development Review Criteria

Appendix - Project Scoring Template

Topic	Criteria	Possible Points	Scored Points
1. Design Guidelines	Proposal corresponds to Design Review Guidelines (DRG)	30	0
	Plan somewhat/partially consistent with DRG	10	
	Plan not consistent with DRG	0	
	Plan has a diversity of types/units	5	
	Plan does not contain diversity of types/units	0	
2. Context Sensitive	Is the development proposal appropriate in scale or provide a buffer to adjacent residential uses?		0
	Yes	10	
	No	0	
	If this is a case of a re-zoning or variance, does the proposed development have more or less, adverse impact on the existing surrounding land uses than development that would otherwise be allowed by right on the same property		
	Less impact	10	
	About the same	0	
	More impact	-10	
3. Land Use Compatibility	Is the development's parking appropriately located to ensure easy walkability to residences, does not form a barrier between sidewalk and any first-floor commercial space in mixed-use developments, and is screened from abutting uses?		0
	Yes	10	
	Partially	5	
	No	0	
4. Beneficial Impacts	Is there inherent conflict with abutting uses (e.g., placing residential adjacent to heavy industrial or heavy trucking)?		0
	Yes	-30	
5. Consistent with Vision	Are there defined beneficial impacts on nearby uses?		0
	Yes	10	
6. Impact Mitigation	Summary evaluation of impact mitigation based upon chart: City has adequate public infrastructure capacity	0	0
	Developer has proposed to undertake mitigation of inadequate infrastructure to enable development	20	
	Some development mitigation provided	5	
	Developer has not proposed sufficient mitigation for inadequate infrastructure issues	-30	
7. Housing Tenure	Are there defined beneficial impacts on nearby uses?		0
	Yes	10	
	No	0	
8. Development Benefits	Is the proposed development consistent, inconsistent or in conflict with a recent local planning effort?		0
	Consistent	10	
	Inconsistent	-10	
	In conflict with	-30	
	No recent planning in area	0	
	Are there significant benefits to the City proposed as part of the development?		
	Significant	10	
	More limited	5	
	None or very limited	0	
	Existing/historic structure or iconic neighborhood feature: The development is incorporating and restoring a historic structure in the development, or is moving the structure to be restored elsewhere in the City	30	
The development is removing an existing non-historic structure in the development, or is moving the structure to be restored elsewhere in the City	15		
The development is preserving an iconic neighborhood feature (e.g., large specimen street tree) as an element of the proposed development	15		
There is not an existing structure on the lot	0		
The development is destroying a historic structure	-30		
9. Affordable Housing	The development is incorporating significant renewable energy elements and/or is incorporating design elements that would qualify the structure for LEED certification	10	0
	The developer would not qualify for LEED certification, but is utilizing the "most efficient available" options for heating and cooling based upon infrastructure in that area of the City	5	
	No significant energy elements in proposed development	0	
	Are the required affordable units: Within the proposed development	30	
10. Affordable Housing (40B)	Off site	20	0
	In cash payments at/near \$50,000/unit	5	
	In cash payments per calculation	10	
	As an alternative to #9 above: If the developer is proposing a zoning change, is the proposed development one that is a "friendly 40B" such that units will be countable on the City's Subsidized Housing Inventory?		
Yes	30	0	
No	0		
		SUM OF POINTS	0
		Maximum Possible	195
		Minimum Possible	-130



# IN CITY COUNCIL

Marlborough, Mass., \_\_\_\_\_ JANUARY 8, 2018

## ORDERED:

At a regular meeting of the Marlborough City Council held on Monday, JANUARY 8, 2018 at 8:00 PM in the City Council Chambers, City Hall, the following proposed amendment to the Code of the City of Marlborough, was **ORDERED ADVERTISED** as follows:

Be it ordained by the City Council of the City of Marlborough that the Code of the City of Marlborough, as most recently amended, be further amended by adding a new Section 650-37 as follows:

### ARTICLE VI

#### §650-37 – RAIL TRAIL OVERLAY DISTRICT

##### A. Purpose and Objectives

- (1) The Assabet River Rail Trail Overlay District (herein, the “ARRTOD”) allows the application of supplemental land use controls within the boundaries of a certain overlay district as an alternative to land use controls that exist in the underlying district(s). The establishment goals of the ARRTOD are to enhance land use development and encourage desired growth patterns for the benefit of the public health, safety and welfare, by promoting pedestrian and bicycle friendly development complimentary to the Assabet River Rail Trail.
- (2) For the purposes of this section, the ARRTOD shall be superimposed on the other districts existing at the time of the adoption of this Section. The ARRTOD consists of (i) the Assabet River Rail Trail property, and (ii) the parcels identified in Exhibit A annexed hereto and incorporated by reference herein.

##### B. Authority of Permit Granting Authority

- (1) The City Council shall be the Permit Granting Authority for Special Permit Approval in the ARRTOD. In all instances, a development which proceeds under the ARRTOD is subject to Site Plan Approval in accordance §270-2 of the Marlborough City Code.



## IN CITY COUNCIL

Marlborough, Mass., JANUARY 8, 2018

PAGE 2

### ORDERED:

- (2) The City Council may, by special permit in accordance with Section 650-59, permit a "Rail Trail Development", defined in Subsection C, on any lot or combination of lots located in the ARRTOD (a "Development Parcel") provided that the Rail Trail Development compliments the Assabet River Rail Trail by improving the appearance of the Development Parcel as seen from the Assabet River Rail Trail, encourages the use of the Assabet River Rail Trail, and provides certain public amenities (including, but not limited to, landscaping, trash receptacles, public safety enhancements, or other benefits) that enhance the Assabet River Rail Trail, either at the Development Parcel or, in the discretion of the City Council, at other locations along the Assabet River Rail Trail.

### C. Eligible Uses, Dimensional Controls:

- (1) A Rail Trail Development shall include any use allowed in the underlying zoning district, either as of right or by special permit, in addition to the following:
  - a. Retail sales and services, up to 5,000 square feet of gross floor area per establishment.
  - b. Restaurant, café with or without table service (including outside seating and service) without drive-thru, up to 5,000 square feet of gross floor area per establishment.
  - c. Residential uses, with such density as seemed appropriate by the City Council.
- (2) A Rail Trail Development may include multiple uses, which may be commingled into a single structure or structures or may be located in multiple separate structures on one building lot.
- (3) A Rail Trail Development shall be subject to the dimensional, parking and landscaping requirements of the underlying zoning district and for the use or combination of uses of the Rail Trail Development, provided, however, that the City Council may elect to vary dimensional, lot coverage, and parking requirements by Special Permit if, in the City Council's opinion, such change shall result in an improved project and will not nullify or substantially derogate from the intent or purpose of this Section. This authority shall continue subsequent to initial occupancy.





# IN CITY COUNCIL

Marlborough, Mass., \_\_\_\_\_ JANUARY 8, 2018

PAGE 3

ORDERED:

## EXHIBIT A

The newly established Assabet River Rail Trail Overlay District shall include all or portions of the properties shown on the Zoning Map existing at the passage of this Ordinance, which properties include the following parcels of land:

1. The Assabet River Rail property, from the Marlborough-Hudson municipal boundary line to the intersection of Highland and Lincoln Streets.
2. Assessors Map 43, Parcel 29 (19 Ash Street).
3. Assessors Map 56, Parcel 125 (269 Mechanic Street).
4. Assessors Map 56, Parcel 96 (3-7 Longley Street).
5. Assessors Map 56, Parcel 85 (297 Lincoln Street).
6. Assessors Map 69, Parcel 345 (283 Lincoln Street).
7. Assessors Map 69, Parcels 340, 341, 342 and 343 (293 Lincoln Street).

Be and is herewith **SET A PUBLIC HEARING FOR MARCH 5, 2018, ADVERTISE, REFER TO URBAN AFFAIRS COMMITTEE AND PLANNING BOARD.**

ADOPTED

ORDER NO. 18-1007137  
X17-1006983A

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**MIRICK O'CONNELL**

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ATTORNEYS AT LAW

RECEIVED  
CITY CLERK'S OFFICE  
CITY OF MARLBOROUGH

2018 JAN -3 P 4:31

**Brian R. Falk**  
Mirick O'Connell  
100 Front Street  
Worcester, MA 01608-1477  
bfalk@mirickoconnell.com  
t 508.929.1678  
f 508.983.6256

January 3, 2018

Councilor Edward Clancy, President  
Marlborough City Council  
City Hall  
Marlborough, MA 01752Re: Proposed Zoning Amendment – Rail Trail Overlay District

Dear Councilor Clancy:

I represent David Skarin, the owner of land located at 19 Ash Street, Assessors Map 43, Parcel 29. On behalf of Mr. Skarin, I respectfully request that the City Council consider amending the Zoning Ordinance of the City of Marlborough by adding a new Section 650-36, Rail Trail Overlay District, as specified in the enclosed Proposed Order.

A similar petition was submitted to the Council in July of 2017, with a public hearing on September 11, 2017. However, that petition did not receive action from the Council (favorable or unfavorable) before the expiration of the last Council session. Further, this new proposal reflects feedback from the Council and MAPC.

The amendment would allow modified use and dimensional requirements for properties within the new Overlay District, consisting of the Rail Trail property itself and certain adjacent parcels (more limited in scope than the original proposal). These "Rail Trail Developments" would be required to improve the appearance of the property as seen from the Rail Trail, encourage the use of the Rail Trail, and provide certain public amenities that enhance the Rail Trail. In accordance with M.G.L. c. 40A, § 5, Mr. Skarin is the owner of land to be affected by this proposed amendment.

Please refer this matter to the Planning Board and take the appropriate steps for review by the City Council.

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**MIRICK, O'CONNELL, DEMALLIE & LOUGEE, LLP**

WORCESTER | WESTBOROUGH | BOSTON

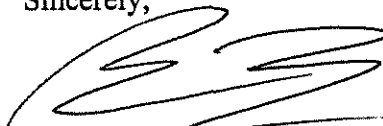
www.mirickoconnell.com

MIRICK O'CONNELL

Marlborough City Council  
January 3, 2018  
Page 2

Thank you for your time and attention to this matter.

Sincerely,

A handwritten signature in black ink, appearing to read 'BRF', with a horizontal line underneath.

Brian R. Falk

BRF/ljk

cc: Client  
Arthur P. Bergeron



## PROPOSED CITY COUNCIL ORDER

Be it ordained by the City Council of the City of Marlborough that the Code of the City of Marlborough, as most recently amended, be further amended by adding a new Section 650-36 as follows:

### ARTICLE VI

#### §650-36 – RAIL TRAIL OVERLAY DISTRICT

##### A. Purpose and Objectives

- (1) The Assabet River Rail Trail Overlay District (herein, the “ARRTOD”) allows the application of supplemental land use controls within the boundaries of a certain overlay district as an alternative to land use controls that exist in the underlying district(s). The establishment goals of the ARRTOD are to enhance land use development and encourage desired growth patterns for the benefit of the public health, safety and welfare, by promoting pedestrian and bicycle friendly development complimentary to the Assabet River Rail Trail.
- (2) For the purposes of this section, the ARRTOD shall be superimposed on the other districts existing at the time of the adoption of this Section. The ARRTOD consists of (i) the Assabet River Rail Trail property, and (ii) the parcels identified in Exhibit A annexed hereto and incorporated by reference herein.

##### B. Authority of Permit Granting Authority

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5. Assessors Map 56, Parcel 85 (297 Lincoln Street).
6. Assessors Map 69, Parcel 345 (283 Lincoln Street).
7. Assessors Map 69, Parcels 340, 341, 342 and 343 (293 Lincoln Street).





# IN CITY COUNCIL

Marlborough, Mass., \_\_\_\_\_ MARCH 5, 2018

PAGE 1

## ORDERED:

That the PUBLIC HEARING On the Proposed Zoning Amendment, adding the Assabet River Rail Trail Overlay District, (ARRTOD), Order No. 18-1007137, X17-1006983, all were heard who wish to be heard, hearing closed at 8:21 PM.

**Councilors Present: Delano, Doucette, Dumais, Tunnera, Irish, Clancy, Landers, Juaire, Ossing & Robey.**

**Absent: Oram.**

## PUBLIC SPEAKING IN FAVOR

Arthur Bergeron, represents David Skarin and his family, the owner of one of the properties affected by this proposed zoning amendment. Mr. Bergeron appeared on behalf of the same clients the previous fall. They own the property on the corner of Ash Street and the Rail Trail where they ran a landscaping business for many years, it was non-conforming to anything else in the area. Their proposal would create a district that would include all the properties along the rail trail and give the City Council the discretion regarding some of the old buildings to approve changes making them more appropriate to the rail trail. The concept of a rail trail did not exist in 1969 when the zoning for the area was created.

Mr. Bergeron responded to comments made from the previous public hearing regarding the ideas for the Skarin property but had concerns about some of the other properties included such as the Universal Aluminum Building. Mr. Bergeron had a meeting with the Metropolitan Area Planning Council (MAPC) and a recommendation from Mark Racicot was to have the rail trail itself be the core but only include in the overlay district those properties they wished to include and that is proposal for this zoning amendment.

Within the district, there were three additional things they could do in addition to the underlying zoning:

1. Housing – only by special permit at a density the City Council deems appropriate;
2. Retail – business or service, no more than five-thousand square feet consistent with or adding to the rail trail; and
3. Restaurant – no more than five-thousand square feet and no drive-through allowed.

The City Council would have the option to approve additional properties into the overlay district and Mr. Bergeron reviewed the five properties he recommended for inclusion. He was not proposing any uses be removed, but requested a special permit be considered for the five properties included in the overlay district for the three additional uses.



# IN CITY COUNCIL

Marlborough, Mass., \_\_\_\_\_ MARCH 5, 2018

PAGE 2

## ORDERED:

Joseph Bisol and his wife Catherine were property owners at 212 Hudson Street and a primary abutter to 19 Ash Street. He spoke to voice their approval of the plan for 19 Ash Street. Mr. Bergeron met with them to explain the changes from the original proposal and now that they understood the additional usage for overlay zoning, it met with their approval. Mr. Bisol voiced his concern with the lack of a master plan for that entire area especially near Lincoln and Mechanic Streets. He requested the Council consider development of the old gas station property, maybe as anchor for the rail trail itself. Currently, the meals tax is used for athletic fields, maybe some future funds could be earmarked for its development which he considered an attractive use for that end of the property.

David Skarin, owner of 19 Ash Street, spoke in favor of the proposal. The project would help beautify the rail trail and it would be a legacy to leave the City of Marlborough. Mr. Skarin respectfully requested the City Council move forward with this project.

There is no one else speaking in favor. That part of the Public Hearing is closed.

## QUESTIONS FROM THE PUBLIC

There are no questions from the public. That part of the Public Hearing is closed.

## PUBLIC SPEAKING IN OPPOSITION

There is no one speaking in opposition. That part of the Public Hearing is closed.

## QUESTIONS FROM THE CITY COUNCIL

✓ Councilor Ossing asked when they appear in Urban Affairs Committee to explain why this proposal was permissible as it looked like spot zoning. Mr. Bergeron stated he would go into the details he received from Mr. Racicot of MAPC in committee.

✓ Councilor Robey shared Councilor Ossing's concerns. She explained how she marked a map of the rail trail zoning for the properties between Ash Street and Lincoln Street and it seemed bizarre the rail trail could be used to connect the properties. She did appreciate the clarification on the new proposed zoning limiting it versus the previous submission where the overlay affected all properties along the rail trail. She reviewed some wording from the proposed zoning, "any use allowed in the underlying zoning district, either as of right or by special permit. in addition to the" three uses as stated by Mr. Bergeron. The current underlying zoning district for the rail trail are A3, RB, CA, and LI. Within all of those, there are many things allowed either by right or special permit that are not a restaurant or housing. She stated Mr. Skarin can currently have his project under a comprehensive permit as the zoning is there. Regarding the remainder of the properties, she was not sure of the need. Mr. Skarin has the desire for his property, but what of the other properties included in this zoning?



# IN CITY COUNCIL

Marlborough, Mass., \_\_\_\_\_ MARCH 5, 2018

PAGE 3

## ORDERED:

Mr. Bergeron explained the wording does not increase the ability to do anything other than what they are allowed per the underlying zoning. Mr. Bergeron would clarify the wording when they appeared before the Urban Affairs Committee.

✓ Councilor Irish stated the property at 269 Mechanic Street have not been a very good neighbor over the years and their driveway could be encroaching on adjacent properties to their lot. It was one building Councilor Irish was not happy to see on this proposal because they have not been a good neighbor.

✓ Councilor Landers asked Mr. Bergeron's opinion on whether this change would motivate other property owners, such as the Johnson-Claflin property, to make changes along the rail trail. Mr. Bergeron explained the residential component drives the development, however they cannot do residential at that property because the underlying zoning is Commercial Automotive. The Johnson-Claflin property would be an ideal residential piece because it is at the beginning of the rail trail. In 1969 when the City's zoning was introduced, the concept of a rail trail was nonexistent.

There are no further questions from members of City Council. That part of the Public Hearing is closed.

That ends the entire Public Hearing. This is currently in the Urban Affairs Committee.

ADOPTED

ORDER NO. 18-1007137A  
X17-1006983A