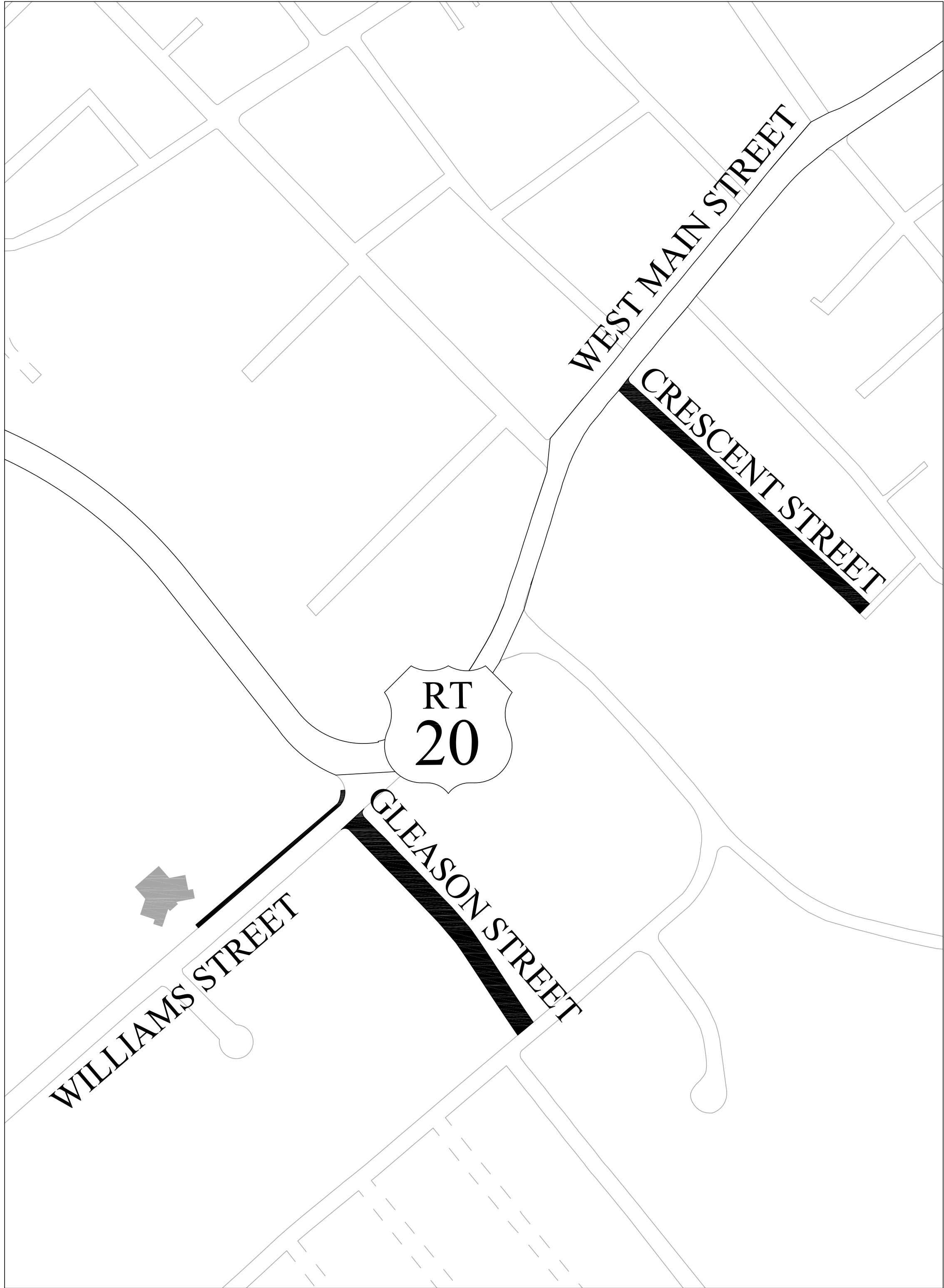


# RECONSTRUCTION OF VARIOUS STREETS & APPURTENANT WORK

COVER SHEET-----	1
CONSTRUCTION NOTES ----- & LEGENDS	2
TYPICAL SECTIONS -----	3
GLEASON STREET ----- CONSTRUCTION PLANS	4-5
CRESCENT STREET ----- CONSTRUCTION PLANS	6-7
FRANCIS STREET ----- CONSTRUCTION PLANS	8-9
TYPICAL DETAILS -----	10-13
TRAFFIC MANAGEMENT PLAN -----	14



PREPARED BY: City of Marlborough  
Department of Public Works  
Engineering Division  
135 Neil Street  
Marlborough, MA 01752

LOCUS  
1"=100'  
**CONTRACT NO. ED 2023-12**  
**MAY 18, 2023**

UNLESS OTHERWISE PROVIDED FOR IN THE PROJECT SPECIFICATIONS, THE FOLLOWING DOCUMENTS SHALL GOVERN THE CONSTRUCTION ON THIS PROJECT:

- THE 2023 MHD STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES....AS AMENDED
- THE 2006 PROJECT DEVELOPMENT & DESIGN GUIDE....AS AMENDED
- THE OCTOBER 2017 CONSTRUCTION STANDARD DETAILS HIGHWAY DIVISION AND ANY AMENDMENTS THERETO
- THE AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1)

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\*NOTE: EXISTING INFRASTRUCTURE AND HATCHES ARE SHOWN IN LIGHT GRAY ON CONSTRUCTION PLANS

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STREET, AND FRANCIS STREET ROAD PAVEMENT NOTES

SURFACE: 4.25" HOT MIX ASPHALT  
(1.75" SUPERPAVE SURFACE COURSE - 9.5 (SSC-9.5))  
(2.5" SUPERPAVE INTERMEDIATE COURSE - 19.0 (SIC-19.0))

ASPHALT EMULSION FOR TACK COAT AT 0.05 GAL/SY OVER INTERMEDIATE COURSE

PROPOSED HOT MIX ASPHALT DRIVEWAY

SUB-BASE: GRAVEL BORROW TYPE C

SURFACE: 3" HOT MIX ASPHALT  
(1.5" SUPERPAVE SURFACE COURSE - 9.5 (SSC-9.5))  
(1.5" SUPERPAVE INTERMEDIATE COURSE - 12.5 (SIC-12.5))

PROPOSED CEMENT CONCRETE SIDEWALK

SUB-BASE: 8" GRAVEL BORROW TYPE C

SURFACE: 6" CEMENT CONCRETE  
AIR ENTRAINED 4000 PSI,  $\frac{3}{4}$ ", 610

---

2. EXISTING UTILITIES, WHERE SHOWN HEREON, ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPERLY LOCATING AND COORDINATING ANY ON-SITE ACTIVITY WITH DIG-SAFE AND THE APPROPRIATE UTILITY COMPANY AND MAINTAINING EXISTING UTILITY SYSTEM SERVICE. DIG-SAFE SHALL BE NOTIFIED PER THE COMMONWEALTH OF MASSACHUSETTS STATUTE CHAPTER 82, SECTION 40, AT 1-888-344-7233. NO GUARANTEE IS IMPLIED OR INTENDED AS TO THE ACCURACY, LOCATION OR THAT ALL UTILITIES AND/OR SUBSURFACE STRUCTURES ARE SHOWN. THE CONTRACTOR SHALL VERIFY SIZE, LOCATION AND INVERTS OR UTILITIES AND STRUCTURES AS REQUIRED PRIOR TO THE START OF CONSTRUCTION.

4. THE CONTRACTOR SHALL ALTER THE MASONRY OF THE TOP SECTION OF ALL EXISTING DRAINAGE AND SEWER STRUCTURES AS NECESSARY FOR CHANGES IN GRADE AND RESET ALL WATER, SEWER AND DRAINAGE FRAMES, GRATES AND BOXES TO THE PROPOSED FINISH GRADE. STRUCTURE ADJUSTMENTS SHALL REQUIRE THE USE OF RED CLAY BRICK CONFORMING TO M4.05.2 AND CEMENT CONCRETE CONFORMING TO M4.02.0 OF THE COMMONWEALTH OF MASSACHUSETTS CONSTRUCTION STANDARDS FOR HIGHWAY AND BRIDGES AS MOST RECENTLY AMENDED.

6. TREES AND SHRUBS WITHIN THE LIMITS OF GRADING SHALL BE REMOVED ONLY UPON APPROVAL OF THE CITY ENGINEER

8. THE TERM "PROPOSED" (PROP.) MEANS WORK TO BE CONSTRUCTED USING NEW MATERIALS OR WHERE APPLICABLE, AND APPROVED BY THE CITY ENGINEER, RE-USING EXISTING MATERIALS SUCH AS THOSE IDENTIFIED AS "REMOVE AND RESET" (R&R).

10. CONTRACTOR IS RESPONSIBLE FOR PROPER REMOVAL AND DISPOSAL OF ALL EXCESS MATERIALS SUCH AS PIPES AND SIGNS NOT PROPOSED FOR REUSE ON THIS PROJECT AT NO ADDITIONAL COST TO THE OWNER.

11. IT IS CRITICAL THAT THE CONTRACTOR STAY WITHIN THE RIGHT-OF-WAY LAYOUT OR LIMIT OF WORK AS SHOWN ON THE DRAWINGS. UTILITY SERVICE CONNECTIONS TO HOUSES AND BUSINESSES ARE NOT SHOWN OR ARE APPROXIMATES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING REPAIRS MADE TO ANY UTILITY SERVICES DISTURBED DURING EXCAVATION AT NO ADDITIONAL COMPENSATION.

12. JOINTS BETWEEN NEW HOT MIX ASPHALT, ROADWAY PAVEMENT, AND THE LOCATIONS OF SAW CUT FOR EXISTING PAVEMENT SHALL BE SEALED WITH HOT APPLIED ASPHALTIC CRACK FILLER.

13. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE NECESSITY OF MAKING HIS OWN INVESTIGATION IN ORDER TO ASSURE THAT NO DAMAGE TO EXISTING STRUCTURES, DRAINAGE LINES, TRAFFIC SIGNAL CONDUITS, ETCETERA, WILL OCCUR. THE CONTRACTOR SHALL NOTIFY MASSACHUSETTS DIG SAFE AND PROCURE A DIG SAFE NUMBER FOR EACH LOCATION PRIOR TO DISTURBING EXISTING GROUND IN ANY WAY. NO EXISTING PUBLIC UTILITY STRUCTURES SHALL BE ABANDONED AND/OR DISMANTLED WITHOUT AUTHORIZATION FROM THE ENGINEER.

14. GAS MAINS AND TELEPHONE LINES ARE ASSUMED TO HAVE FEET OF COVER UNLESS NOTED OTHERWISE. WATER MAINS ARE ASSUMED TO HAVE 5 FEET OF COVER UNLESS NOTED OTHERWISE. IT IS NOT WARRANTED THAT ALL UTILITIES ARE SHOWN OR THAT THEY ARE PLACED AT THESE ASSUMED DEPTHS. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE RELOCATION OF ANY CONFLICTING UTILITIES WITH THE RESPECTIVE UTILITY COMPANY. RELOCATION OF UTILITIES AND/OR THEIR APPURTENANCES REQUIRED TO ACCOMMODATE THE NEW WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. FOR THE COST INFORMATION CONCERNING RELOCATION OF UTILITIES SEE SPECIFICATIONS GENERAL CONDITIONS 25 NOTICE TO UTILITIES.

15. ALL EXISTING STATE, COUNTY, CITY AND TOWN LINES AND PRIVATE PROPERTY LINES HAVE BEEN ESTABLISHED FROM AVAILABLE INFORMATION AND THEIR EXACT LOCATION ARE NOT GUARANTEED.

16. ALL UNDERGROUND UTILITY WORK MUST BE COMPLETED AND TEMPORARILY PAVED AT THE END OF EACH WEEK OR PRIOR TO ANY RAIN EVENTS UNLESS OTHERWISE APPROVED BY THE ENGINEER.

17. THE CONTRACTOR SHALL PROVIDE FOR SAFE AND ORDERLY PASSAGE OF VEHICULAR AND PEDESTRIAN TRAFFIC IN AREAS UNDER CONSTRUCTION.

18. ALL EXISTING CONCRETE, HMA WALKS OR PAVERS TO ABUTTING PROPERTIES SHALL BE REMOVED AND REPLACED IN KIND AS REQUIRED TO TRANSITION TO PROPOSED GRADES.

19. BACKFILL SHALL BE UNIFORMLY DISTRIBUTED IN SUCCESSIVE LAYERS, EACH LAYER BEING THOROUGHLY COMPACTED BEFORE THE SUCCEEDING LAYER IS PLACED. THE ENTIRE WIDTH OF THE TRENCH SHALL BE MECHANICALLY OR HAND TAMPED IN SIX (6) INCH LIFTS, A MINIMUM OF TWO (2) FEET ABOVE UTILITY INSTALLATION, AND MECHANICALLY TAMPED THE REMAINDER OR THE FILL IN LIFT DEPTHS NOT GREATER THAN SIX (6) INCHES. THE TOP LAYER OF BACKFILL SHALL BE FOURTEEN (14) INCHES (IN A TEMPORARY TRENCH), AND TWELVE (12) INCHES (IN A PERMANENT TRENCH) OF GRAVEL COMPACTED TO 97% OF MAXIMUM DENSITY.

20. ALL STRUCTURES TO BE REMODELED UNLESS OTHERWISE NOTED.

21. ALL SEEDING WORK SHALL BE CARRIED OUT BY AN APPROVED SPRAYING MACHINE SPECIFICALLY USED FOR THIS WORK AND NOT PLACED BY HAND.

22. ALL DRIVEWAYS SHALL BE SAW-CUT AT BLENDING LIMITS SHOWN, OR AS DIRECTED BY THE ENGINEER.

23. ALL MAILBOXES TO BE REMOVED & RESET PER DETAIL. SEE SHEET. SLEEVES ARE TO BE LEFT FOR ALL MAILBOXES CURRENTLY ON HOUSES. WORK IS TO BE CONSIDERED INCIDENTAL TO ALL WORK.

24. THE SURFACE OF CEMENT CONCRETE SIDEWALK AND PEDESTRIAN CURB RAMP PANELS SHALL BE BROOM FINISHED AND BORDERED WITH A 1.5" FINISHING TROWEL.

25. MULCH FOR BLENDING DISTURBED AREAS SHALL MATCH EXISTING.

26. ROADWAY GRADES TO BE ADJUSTED TO ENSURE SIDEWALKS MEET ADA REQUIREMENTS.

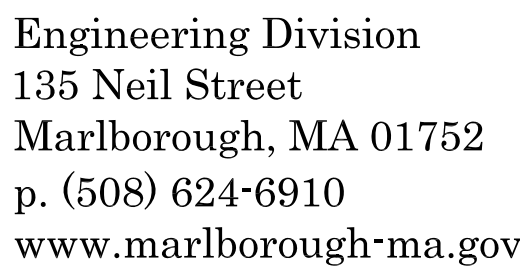
27. SIDEWALK CROSS SLOPES SHALL BE AS SHOWN ON PROPOSED TYPICAL SECTIONS.

28. ALL SERVICE BOXES TO BE ADJUSTED UNLESS OTHERWISE NOTED.

29. ALL WATER GATE BOX TOP SECTIONS TO BE REPLACED.

30. THE CONTRACTOR SHALL COORDINATE WITH EVERSOURCE GAS FOR THE ADJUSTMENT OF GAS GATES & SERVICE BOXES

31. WHERE SHOWN, EROSION CONTROLS SHALL BE ERECTED AND APPROVED BY THE ENGINEER PRIOR TO START OF WORK. CONTRACTOR SHALL ENSURE THAT NO SILT-LADEN RUNOFF LEAVES THE WORK SITE.



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Drawn By: TJS    Designed By: TJS    Checked By: MJD    Approved By: TD

## RECONSTRUCTION OF VARIOUS STREETS & APPURTENANT WORK

## CONSTRUCTION NOTES AND LEGENDS

2

34' LAYOUT

EX. UPOLE

VARYING EXISTING TRAVEL WAY

11' TRAVEL LANE

11' TRAVEL LANE

5.0'

4" HMA LAID IN TWO COURSES (2 1/2" BINDER, 1 3/4" TOP)

3/8" PER FOOT

12" STABILIZED GRAVEL BASE

3/8" PER FOOT

3/8" PER FT.

PROP. INTEGRAL CEM. CONC. SIDEWALK

8" GRAVEL BASE

4" LOAM BORROW & SEED

GRADE TO MEET EXISTING GROUND

2:1 MAX

2:1 MAX

PROP. PRECAST CB W/4" SUMP WITH EX. 12" RCP

EX. 4" STEEL GAS

EX. 6" CLAY SEWER TO BE LINED W/ CIPP BY OTHERS

EX. 8" AC WATER

PROP. PRECAST CB W/4" SUMP

VARIES

VARIES

VARIES

VARIES

40' LAYOUT

VARYING EXISTING TRAVEL WAY

21' TRAVEL WAY

10.5' TRAVEL LANE

10.5' TRAVEL LANE

EX. UPPOLE

PROP. HYDRANT

GRADE TO MEET EXISTING GROUND

2:1 MAX

2:1 MAX

4" LOAM BORROW & SEED

PROP. 5' INTEGRAL CONC. SIDEWALK W/ 8" GRAVEL BASE

PROP. PRECAST CB W/ 4" SUMP W/ 12" CHDPE

12" STABILIZED BASE

4 1/2" HMA LAID IN TWO COURSES (2 1/2" BINDER, 1 1/2" TOP)

3" PER FOOT

3" PER FOOT

4" LOAM BORROW & SEED

GRADE TO MEET EXISTING GROUND

2:1 MAX

2:1 MAX

4" LOAM BORROW & SEED

VARIES

VARIES

VARIES

VARIES

VARIES

VARIES

PROP. PRECAST CB W/ 4" SUMP W/ 12" CHDPE


ACTIVE 2" PLASTIC GAS MAIN

ABANDONED 2" STEEL GAS MAIN

EX. 8" VC SEWER

EX. 8" CI WATER MAIN

**CITY<sup>OF</sup>  
MARLBOROUGH**



**D<sup>E</sup>  
P<sup>U</sup>  
W<sup>O</sup>** DEPARTMENT OF  
PUBLIC  
WORKS

Engineering Division  
135 Neil Street  
Marlborough, MA 01752  
p. (508) 624-6910  
[www.marlbrough-ma.gov](http://www.marlbrough-ma.gov)

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Project Title:

RECONSTRUCTION OF  
VARIOUS STREETS &  
APPURTENANT WORK

Contract No: ED-2023-12	Sheet No.:  3
Date: 5-18-2023	
Scale: N.T.S.	









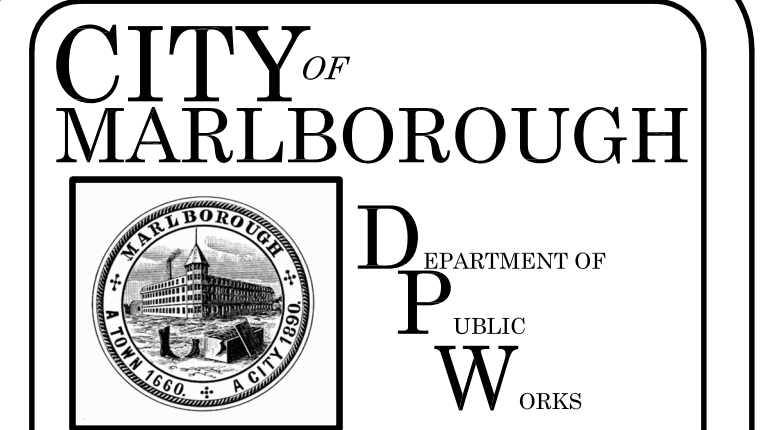


EXISTING CRESCENT ST. STRUCTURES TABLE

STRUCTURE ID	STATION	RIM ELEV.	INV. IN ELEV.	INV. OUT ELEV.	REMARKS
EX. DMH#1	0+00.00	449.22			NEW F&C
EX. CB#1	0+26.11	492.53	(CB#3) 489.20	(DMH#1) 489.20	NEW F&G
EX. CB#2	0+27.54	493.04		(DMH#1) 489.71	NEW F&G
EX. CB#3	1+76.57	504.46	(CB#4) 500.34 (CB#5) 499.82	(CB#1) 499.16	REMOVE
EX. CB#4	1+79.07	504.41		(CB#3) 500.43	REMOVE
EX. SMH#1	2+00.39	506.39	(SMH#2) 501.03	500.85	NEW F&C
EX. CB#5	3+21.33	515.07	(CB#6) 511.87 (CB#7) 511.87	(CB#3) 511.67	REMOVE
EX. CB#6	3+28.70	515.73		(CB#5) 512.13	REMOVE
EX. SMH#2	4+35.46	523.30	(SMH#3) 516.42	(SMH#1) 516.22	REMODEL
EX. SMH#3	5+38.27	528.84	522.54	(SMH#2) 522.34	REMODEL
EX. CB#7	6+28.32	531.40	8" CLAY 527.78	(CB#5) 527.02	REMOVE

### PROPOSED CRESCENT ST. DRAINAGE STRUCTURES TABLE

STRUCTURE ID	STATION	RIM ELEV.	INV. IN ELEV.	INV. OUT ELEV.	REMARKS
PROP. CB #1	1+76.57	504.46	(CB #2) 500.34 (CB #3) 499.82	(EX. CB#1) 499.16	
PROP. CB #2	1+79.07	504.41		(CB #1) 500.43	
PROP. CB #3	3+21.33	515.07	(CB #4) 511.87 (CB #5) 511.87	(CB #1) 511.67	
PROP. CB #4	3+28.70	515.73		(CB#3) 512.13	
PROP. CB #5	6+28.32	531.40	8" CLAY 527.78	(CB#3) 527.02	



Engineering Division  
135 Neil Street  
Marlborough, MA 01752  
p. (508) 624-6910  
[www.marlborough-ma.gov](http://www.marlborough-ma.gov)

## REVISIONS

[illegible]

Drawn By: T.B.    Designed By: T.B.    Checked By: M.D.    Approved By: D.

Project Title:

RECONSTRUCTION OF  
VARIOUS STREETS &  
APPURTENANT WORK

Sheet Title:

## CRESCENT STREET DRAINAGE TABLES

Contract No:  
ED-2023-12

Date: 5-18-2023

Scale: 1"=20'

Sheet No.:

7



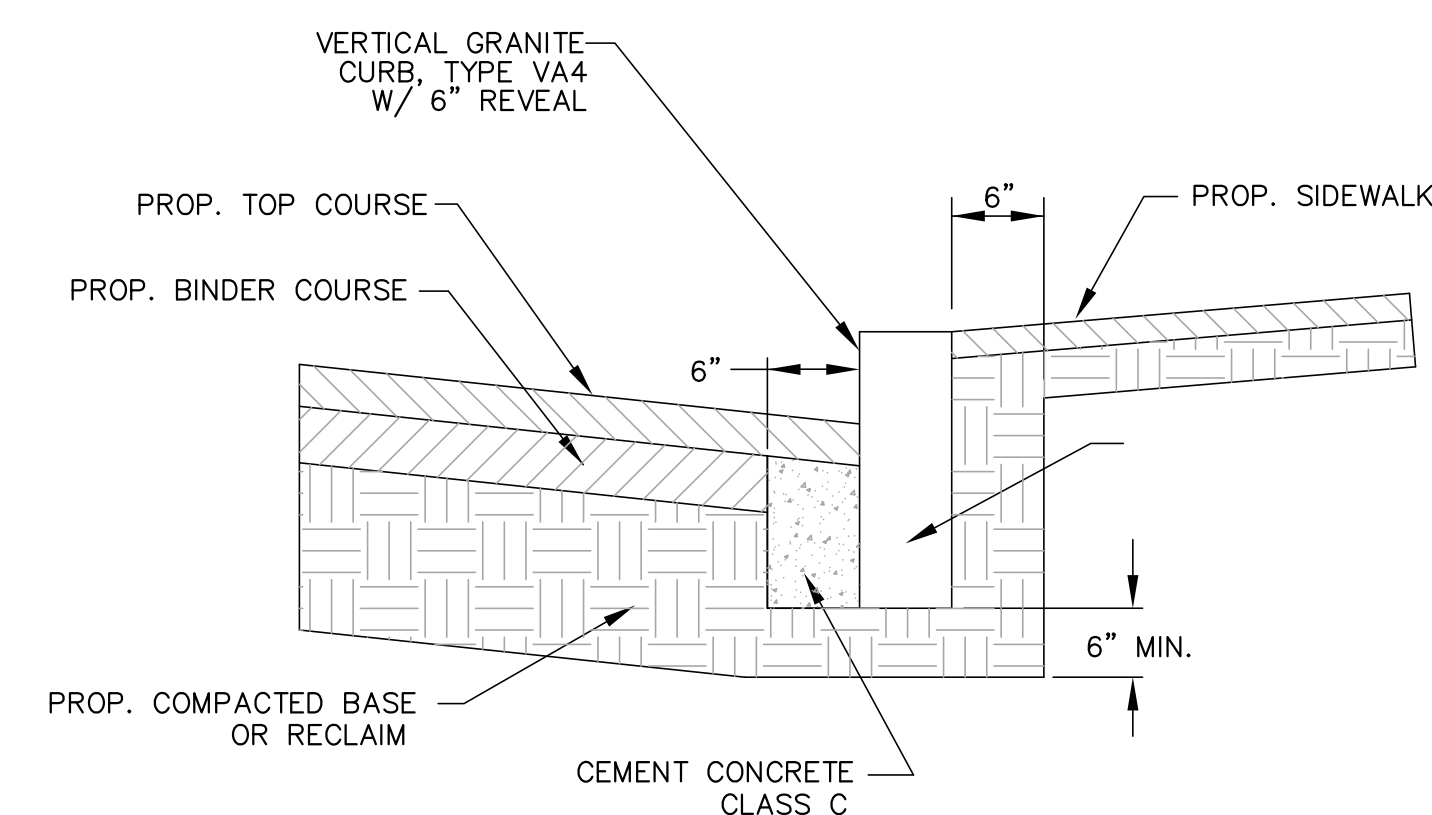
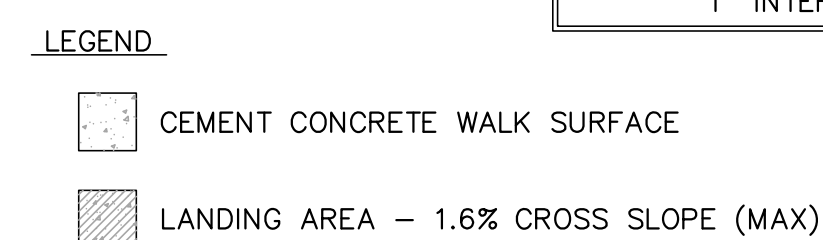
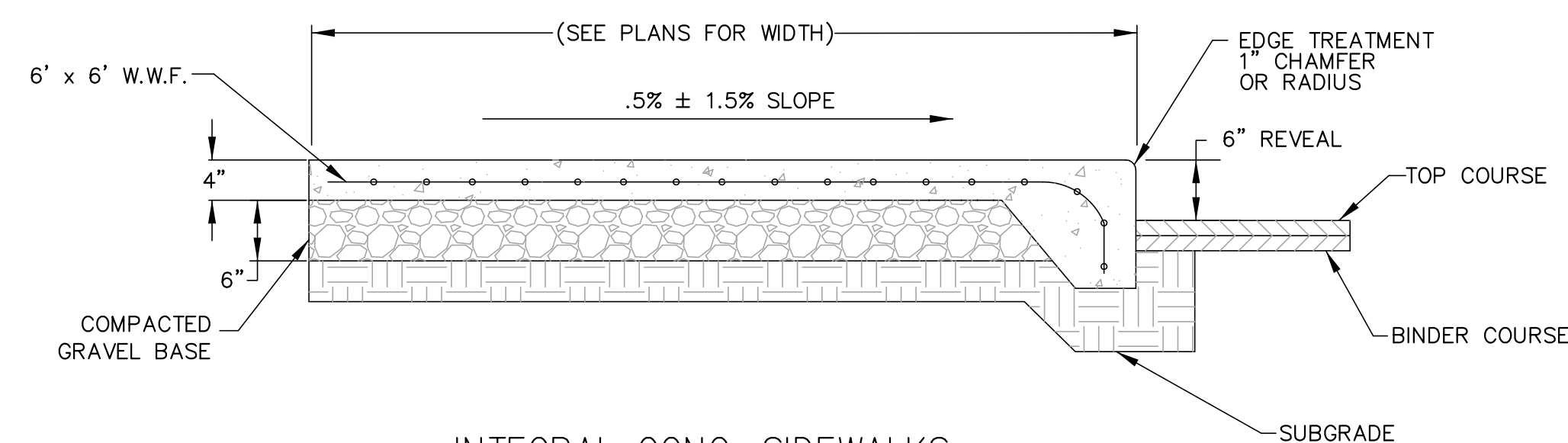
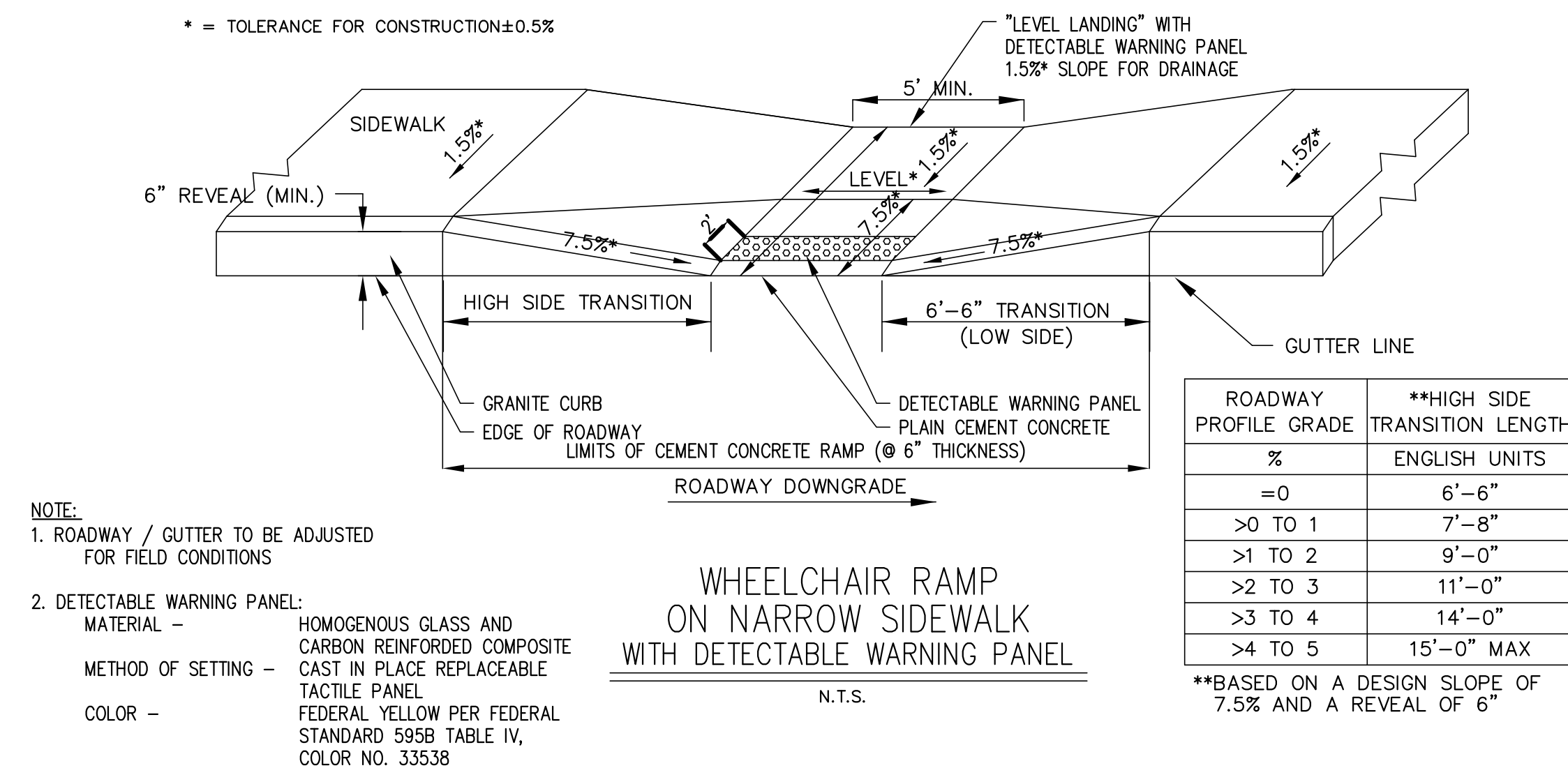
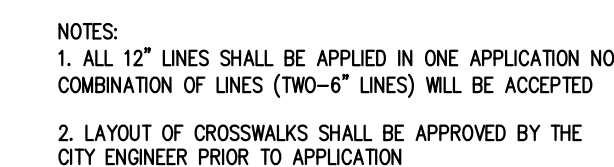






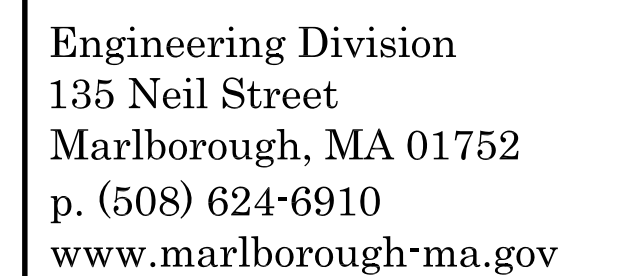






Note:

1) All details are subject to change without notice. Check with the City of Marlborough's Department of Public Works prior to using these details for the most up to date revisions.

REVISIONS[illegible]

Drawn By: 1.5    Designed By: 1.5    Checked By: M.P    Approved By: 12

Project Title:

RECONSTRUCTION OF  
VARIOUS STREETS &  
APPURTENANT WORK

Sheet Title:

## TYPICAL DETAILS

Contract No:  
ED-2023-12

Date:  
5-18-2023

Scale:  
N.T.S.

Sheet No.: 11

THE FOLLOWING DESCRIBES MATERIALS AND METHODS AS THEY PERTAIN TO ROAD OPENING WORK. ALL WORK SHALL CONFORM TO THE 1988 STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, AND ALL SUBSEQUENT SUPPLEMENTAL SPECIFICATIONS THERETO.

EXCAVATIONS IN THE STREET PAVEMENT AREAS SHALL BE CAREFULLY BACKFILLED WITH LAYERS OF SUITABLE GRAVEL. THE TWELVE (12) INCHES OF BEDDING MATERIAL AROUND PIPES SHALL BE SAND (M1.04.0) FOR WATER,  $\frac{3}{4}$ " CRUSHED STONE (M2.01.0) FOR SEWER, GRAVEL BORROW - TYPE C (M1.03.0) FOR RCP DRAIN & TYPE D (M1.03.0) FOR CHDPE DRAIN OR AS SPECIFIED BY THE OWNER OF THE UTILITY AND APPROVED BY THE CITY OF MARLBOROUGH.

BACKFILL MATERIAL SHALL CONSIST OF CONTROLLED DENSITY FILL (CDF) – TYPE 1E OR 2E (M4.08.0) FOR ALL MAJOR ROADWAYS OF THE CITY OR AS DIRECTED BY THE COMMISSIONER OF PUBLIC WORKS. WHERE REQUIRED, CDF SHALL BE PLACED TO WITHIN A MINIMUM OF 5" FROM THE TOP OF THE EXISTING PAVEMENT OR AS DIRECTED BY THE CITY'S INSPECTOR. CDF SHALL BE TYPE 1E VERY FLOWABLE (EXCAVATABLE) OR TYPE 2E FLOWABLE (EXCAVATABLE) WITH A MAXIMUM STRENGTH OF 200 PSI AND 25-30% AIR. CDF (TYPE 1E AND 2E) SHALL BE EXCAVATABLE USING SIMPLE HAND TOOLS WHERE REQUIRED.

BACKFILL SHALL BE UNIFORMLY DISTRIBUTED IN SUCCESSIVE LAYERS, EACH LAYER BEING THOROUGHLY COMPACTED BEFORE THE SUCCEEDING LAYER IS PLACED. THE ENTIRE WIDTH OF THE TRENCH SHALL BACKFILLED AND MECHANICALLY TAMPED IN LIFT DEPTHS NOT GREATER THAN (6) INCHES. COMPACTION EQUIPMENT AND LIFT THICKNESS SHALL MEET OR EXCEED THE MINIMUM REQUIREMENTS TO OBTAIN THE COMPACTION STANDARDS UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER. THE TOP LAYER OF BACKFILL SHALL BE FOURTEEN (14) INCHES OF GRAVEL COMPACTED TO 97% OF MAXIMUM DENSITY.

PAVEMENT PLACED SHALL BE PLACED AND RAKED TO A UNIFORM SURFACE, ROLLED TO THE REQUIRED THICKNESS AND TO A GRADE THAT WILL MATCH THE EXISTING BITUMINOUS ROAD SURFACE. THE PERMITTEE SHALL MAINTAIN THE SURFACING AND SHALL PROMPTLY FILL WITH SIMILAR MATERIAL ANY DEPRESSION AND HOLES THAT MAY OCCUR SO AS TO KEEP THE SURFACING IN A SAFE AND SATISFACTORY CONDITION FOR TRAFFIC.

A TEMPORARY PAVEMENT SHALL BE PLACED ON THE SURFACE OF THE FILL AND THOROUGHLY COMPACTED. A TEMPORARY PAVEMENT SHALL BE HOT MIX ASPHALT NO LESS THAN THE EXISTING BASE COURSE OR A MINIMUM THREE (3) INCHES THICK (SEE EXCAVATION TRENCH DETAIL). IF PLATES ARE USED PRIOR TO TEMPORARY PAVING, THEY SHALL BE RAMPED TO MATCH THE EXISTING PAVEMENT. TEMPORARY PAVING OR STEEL PLATING SHALL IMMEDIATELY FOLLOW THE BACKFILLING OPERATION OR AS DIRECTED BY ENGINEER.

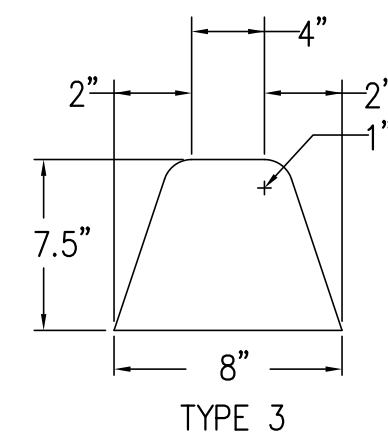
THE FINAL HOT MIX ASPHALT SURFACE SHALL NOT BE PLACED ANY EARLIER THAN 65 DAYS OR NO LATER THAN 90 DAYS FROM THE DATE OF COMPLETION OF THE TEMPORARY SURFACE WITHOUT APPROVAL FROM THE COMMISSIONER OF PUBLIC WORKS. THE TEMPORARY PAVEMENT SHALL BE MILLED TO A DEPTH OF ONE AND ONE-HALF (1  $\frac{1}{2}$ ) INCHES, TWELVE (12) INCHES OUTSIDE OF THE TEMPORARY PAVING LIMITS. PAVEMENT LIMITS JOINTS ARE TO BE SEALED WITH HOT APPLIED HMA CRACK SEALER, PER M3.05.4. IN SOME INSTANCES, FOR PROJECTS BEYOND A SIMPLE TRENCH EXCAVATION ADDITIONAL PAVEMENT DETAILS AND RESURFACING REQUIREMENTS MAY BE INCLUDED AS A SPECIAL CONDITION IN THE PERMIT.

ALL EXCAVATIONS MUST BE PROPERLY SLOPED, SHORED, OR SHIELDED UNLESS THEY ARE MADE ENTIRELY IN STABLE ROCK, OR LESS THAN (5) FEET DEEP AND DECLARED SAFE AFTER AN INSPECTION BY A COMPETENT PERSON.



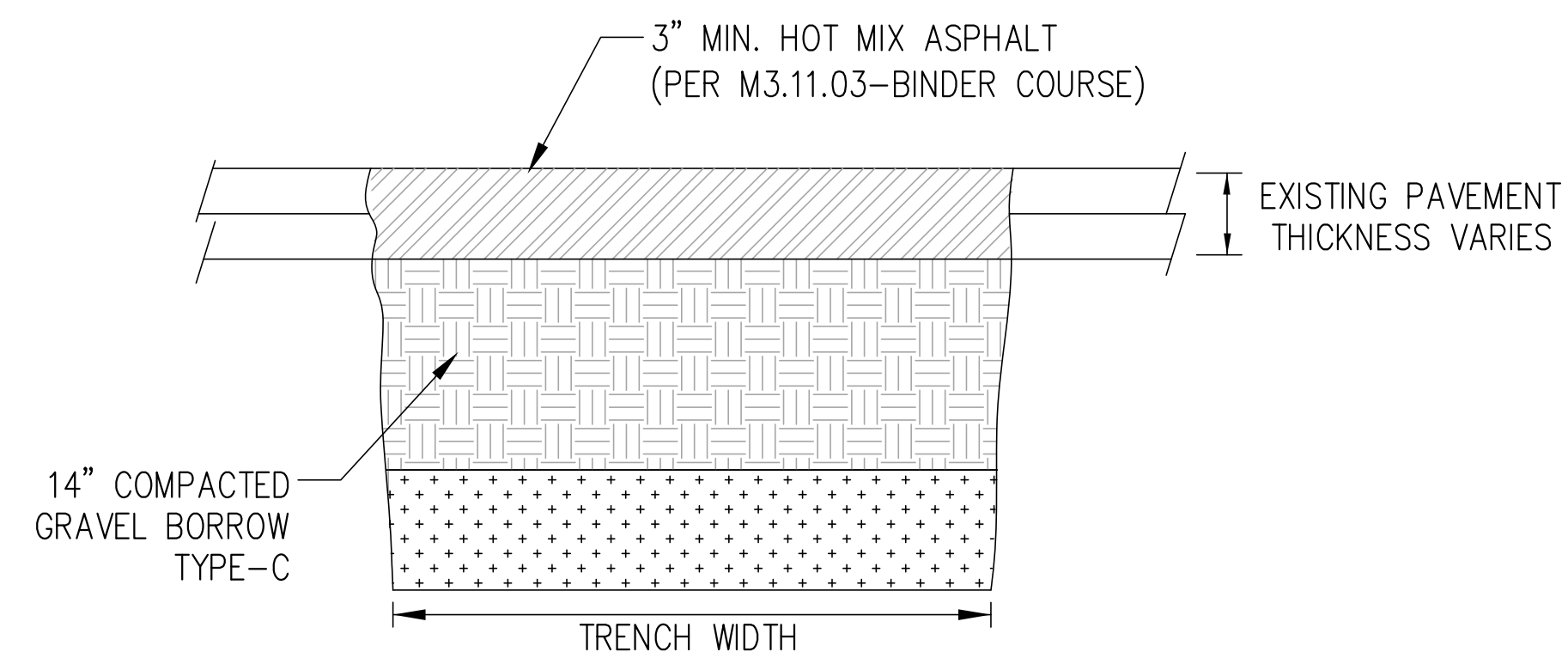
## 12





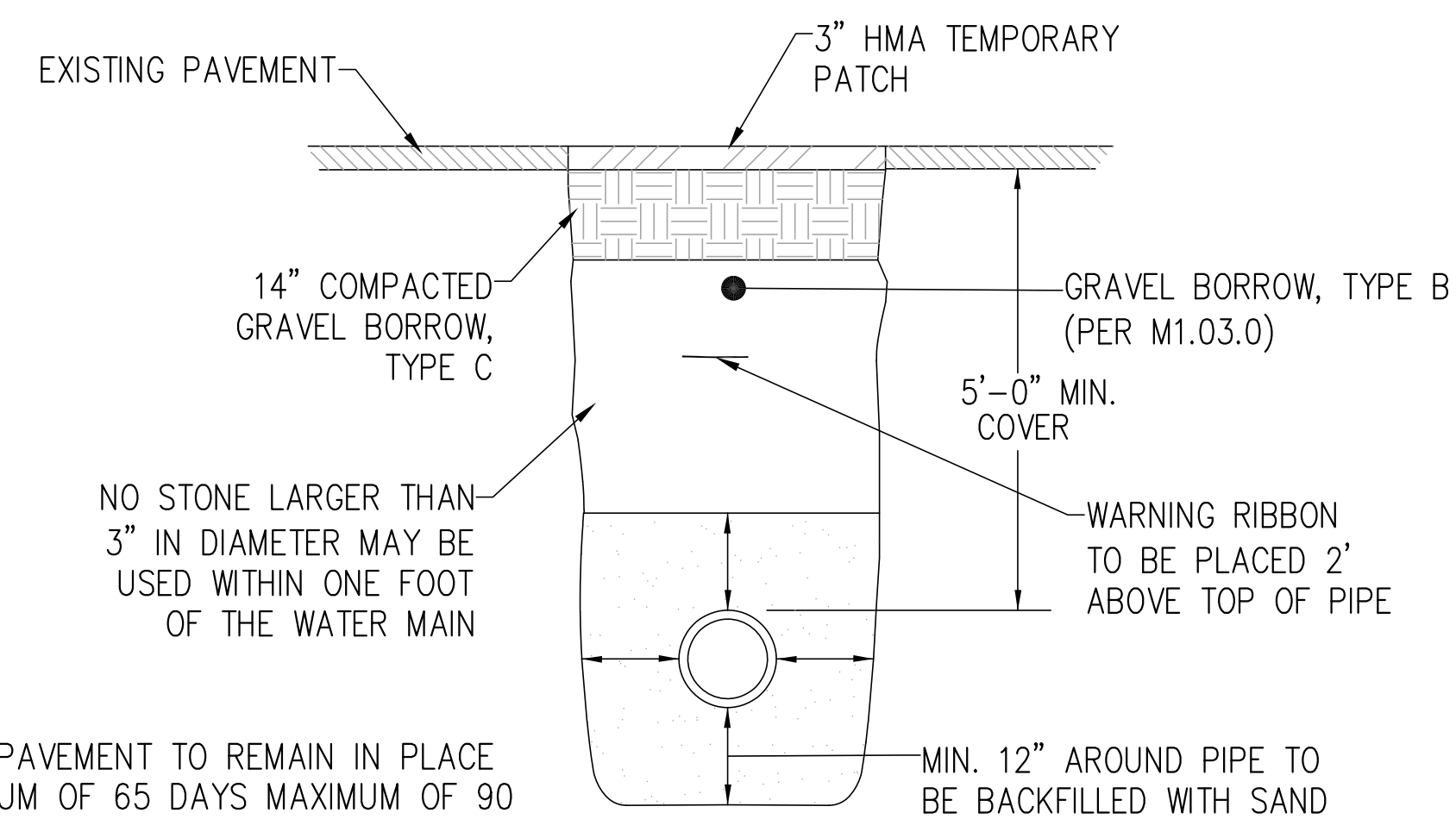
Note:  
1) All details are subject to change without notice. Check with the City of Marlborough's Department of Public Works prior to using these details for the most up to date revisions.

N.T.S.



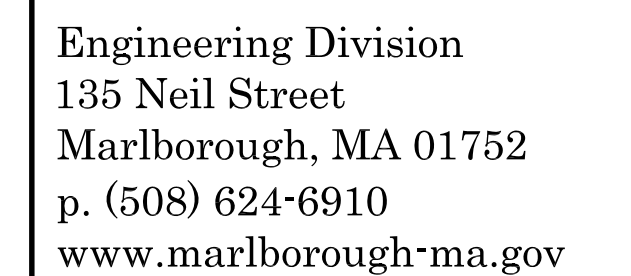
1. COMPACTION EQUIPMENT AND LIFT THICKNESS SHALL MEET OR EXCEED THE MINIMUM REQUIREMENTS TO OBTAIN THE COMPACTION STANDARDS DESCRIBED IN THE CONSTRUCTION NOTES, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER. IN PROXIMITY TO STRUCTURES, A JUMPING JACK COMPACTOR SHALL BE REQUIRED.

N.T.S.



1. TEMPORARY PAVEMENT TO REMAIN IN PLACE FOR A MINIMUM OF 65 DAYS MAXIMUM OF 90 DAYS, TO BE COMPLETELY REMOVED DURING FINAL PAVING.
2. COMPACTION EQUIPMENT AND LIFT THICKNESS SHALL MEET OR EXCEED THE MINIMUM REQUIREMENTS TO OBTAIN THE COMPACTION STANDARDS DESCRIBED IN THE CONSTRUCTION NOTES, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER. IN PROXIMITY TO STRUCTURES, A JUMPING JACK COMPACTOR SHALL BE REQUIRED.

N.T.S.

[illegible]

Drawn By: I.S.    Designed By: I.S.    Checked By: M.D.    Approved By: ID

RECONSTRUCTION OF  
VARIOUS STREETS &  
APPURTENANT WORK

## TYPICAL DETAILS

Contract No: ED-2023-12	Sheet No.:  13
Date: 5-18-2023	
Scale: N.T.S.	

